



# PLANNING DEPARTMENT<sup>1</sup>

P.O. Box 40 • Irrigon, Oregon 97844  
(541) 922-4624 or (541) 676-9061 x 5503  
FAX: (541) 922-34

## AGENDA

**Morrow County Planning Commission**  
**Tuesday, March 29, 2022 7:00 pm**  
**Morrow County Government Building**  
**Irrigon, OR**

**For Electronic Participation See Meeting Information on Page 2 - 3**

### Members of Commission

Jeff Wenholz, Chair  
Stacie Ekstrom  
Greg Sweek

Wayne Seitz  
Rod Taylor

Stanley Anderson  
Karl Smith  
Brian Thompson

### Members of Staff

Tamra Mabbott, Planning Director  
Stephen Wrecsics, GIS Planning Tech  
Katie Keely, Compliance Planner

Stephanie Case, Planner II  
Justin Nelson, County Counsel  
George Nairns, Office Manager

1. Call to Order
2. Roll Call

Pledge of Allegiance: "I pledge allegiance to the flag of the United States of America and to the republic for which it stands: one nation under God, indivisible with liberty and justice for all."

3. Minutes: February 22, 2022
4. Public Hearings to begin at 7:00 pm (COMMISSION ACTION REQUIRED):

**Continued Hearing: Zoning Permit Z-2956-22: Yost Grube Hall Architecture, Applicant. Amazon Data Services, Inc., Owner.** Property is tax lot 1701 of Assessors Map 4N 25E 24, also known as Parcel 1 of Partition Plat 2021-25, located on the west side of Bombing Range Road, one mile south of the Highway 730 and Interstate 84 Interchange. Zoning is General Industrial (MG). Application is for a data center campus, security building and water treatment building. Criteria for approval include MCZO Section 1.050, 3.070 A-E and Article 4 Supplementary Provisions.

**Land Partition LP-N-508-22 and Replat R-N-076-22: Rowan Green Data, Applicant and Threemile Canyon Farms, Owner.** The property is described as Parcel 1 of Partition Plat 2012-3, the portion affected is tax lot 100 of Assessor's Map 3N 24E. The property is zoned Exclusive Farm Use (EFU) located southwest of Boardman on Tower Road. The request is to replat parcel 1 of Partition Plat 2012-3 to create one parcel. Criteria for approval includes the MCZO Article 3 Section 3.010 Exclusive Farm Use Zone and Morrow County Subdivision Ordinance Article 5 Land Partitioning.

**Land Partition LP-N-509-22: Barbara Hug, Applicant and Owner:** The property is described as tax lot 1000 of Assessor's Map 4N 25E 15. The property is zoned Farm Residential (FR) outside the Boardman UGB and located less than ¼ mile east of Boardman on Kunze Lane. The request is to divide

an approximately 9.7-acre parcel into two parcels. Criteria for approval includes MCZO Article 3 Section 3.041 Farm Residential Use Zone and Morrow County Subdivision Ordinance Article 5 Land Partitioning.

**Hardship Variance HV-N-044-22: Glenda Bahler applicant and Gary Klinger owner.** The property is described as Tax Lot 1001 of Assessor's Map 4N 25 15 and is located east of the city of Boardman. The property is zoned Farm Residential outside the Boardman Urban Growth Boundary (UGB). Request is to approve a temporary hardship dwelling to allow care for an infirm person. Criteria for approval include MCZO Article 7 Section 7.300 Special Uses.

5. Correspondence  
Directors Report - February & March
6. Public Comment
7. Adjourn

**Next Meeting:** April 26, 2022 at 7:00 p.m.  
Location: Bartholomew Building, Heppner, OR

#### ELECTRONIC MEETING INFORMATION

Morrow County Planning is inviting you to a scheduled Zoom meeting. Topic: Planning Commission  
Time: March 29, 2022 07:00 PM Pacific Time (US and Canada)

Join Zoom Meeting

<https://us06web.zoom.us/j/3696517452?pwd=dm9kTG1UbWM1OUUpqRDdyVXA1TmU2dz09>

Meeting ID: 369 651 7452

Passcode: 335454

One tap mobile

+12532158782,,3696517452#,,,,\*335454# US (Tacoma)

+13462487799,,3696517452#,,,,\*335454# US (Houston)

Dial by your location

+1 253 215 8782 US (Tacoma)

+1 346 248 7799 US (Houston)

+1 720 707 2699 US (Denver)

+1 301 715 8592 US (Washington DC)

+1 312 626 6799 US (Chicago)

+1 646 558 8656 US (New York)

Meeting ID: 369 651 7452

Passcode: 335454

Find your local number: <https://us06web.zoom.us/j/3696517452?pwd=dm9kTG1UbWM1OUUpqRDdyVXA1TmU2dz09>

**Should you have any issues connecting to the Zoom meeting, please call 541-922-4624. Staff will be available at this number after hours to assist.**

*This is a public meeting of the Morrow County Planning Commission, and may be attended by a quorum of the Morrow County Board of Commissioners. Interested members of the public are invited to attend. The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired, or for other accommodations for persons with disabilities, should be made at least 48 hours before the meeting to Tamra Mabbott at (541) 922-4624, or by email at [tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us).*

**PRELIMINARY FINDINGS OF FACT  
ZONING PERMIT**

Application No. ZP-2956-22

REQUEST: Zoning permit approval for a data center campus including primary and accessory uses and buildings. Request does not include electrical power substation.

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APPLICANT: Yost Grube Hall Architecture  
707 SW Washington Street, Suite 1200  
Portland, OR 97205

LANDOWNERS: Amazon Data Services, Inc.  
410 Terry Avenue North  
Seattle, WA 98109

PROPERTY DESCRIPTION: Tax Lot 1701 of Assessor's Map 4N 25 24, otherwise known as Parcel 1 of Partition Plat 2021-25

PROJECT LOCATION: Property is located approximately one mile (4,750 feet) south of the Highway 730 and Interstate 84 Interchange.

FINDINGS OF FACT:

I. BACKGROUND INFORMATION:

The subject property is 126.92 acres zoned General Industrial.

The application requests zoning permit approval, including a determination of consistency with Morrow County Zoning Ordinance (MCZO) 3.070, for a campus master plan, including the type and general location of all proposed primary and accessory uses/buildings. After obtaining Planning Commission approval of this application, the applicant will, as the site is built out in the future, submit detailed, building specific zoning permit applications which will be reviewed and processed by Planning Staff.

As illustrated on the enclosed site plan, the scope of the proposed master plan development includes four new single story, 215,461 square foot data center buildings, 2,116 square foot industrial water treatment building and associated 490,000-gallon water storage tanks, 6,560 square foot security building, and 14,000 square foot data storage building. Site work includes grading to accommodate a storm water pond for on-site storm water management, septic system for onsite wastewater treatment, drive aisles and parking areas, and generator yard pads.

The Port of Morrow will be constructing a 5-acre industrial waste water holding pond on the site. The pond will be part of a closed system, only accepting water from the data center campus and holding it until it can be used to irrigate surrounding agricultural uses. The holding pond

may be considered an accessory use for the data center development. Other permits may be required for the waste water holding pond. Land application of industrial wastewater is not a part of this request. A land use permit (Land Use Decision MCZO Section 3.010(B) and Subsection D.8 will be required for the land application of industrial wastewater onto lands zoned Exclusive Farm Use. This is listed below as a condition of approval.

The Umatilla Electric Cooperative (UEC) will provide power to the site from an on-site substation. See attached master site plan. The substation is not a part of this land use request. As the dedicated power source for the data center, the substation is an accessory use and may be permitted subject to MCZO Section 3.070(A)(15). According to UEC, the utility currently owns and operates 230kV transmission lines on the East side of Bombing Range Road. To serve this campus, UEC is proposing that upgrades be made to those existing facilities including a line extension starting near the UEC East Wilson Substation across Bombing Range Road to the west and south of the campus. Please see attached ownership map which shows the proximity of the substation to the subject parcel. The proposed transmission line extension, which is not part of this application, is approximately 0.5 miles in length and, according to the applicant, will be designed to minimize impact to private property and EFU. UEC will be required to obtain any right of way and road crossing permits from Morrow County. UEC will be required to secure authorization from the private landowners. UEC will be required to secure land use permits for the new transmission line. These requirements are listed below as conditions of approval.

According to the application, domestic water service including potable, construction, fire suppression, and industrial cooling water will be delivered from the Port of Morrow (POM) east beach water system that is supplied by a combination of municipal water rights from alluvial, basalt, and surface water sources. According to the applicant, the POM has the capacity to provide the service. Most of the 20" potable pipeline will be installed on POM owned property with a couple small sections of needed private easements. POM will be responsible for acquiring the private easements and according to the applicant POM has already begun that process in anticipation of this campus project. Additionally, POM will be responsible for obtaining any required crossing permits from ODOT, UPRR, and Morrow County. These requirements are listed below as conditions of approval.

#### Previous Land Use Approval:

On August 3, 2021, the Planning Commission determined that, pursuant to ORS 197.713, the subject site is eligible for industrial development and construction of buildings of any size and type, including accessory uses and on-site sewer facilities to serve same. See County File LUD-N-38-21.

On August 3, 2021 the Planning Commission approved a tentative partition plat that created the subject property as Parcel 1. See County File LD-498-21. As part of that process, the County determined that public facilities and services were available and adequate to the site. After obtaining approval of the tentative partition plat, the landowner then obtained approval of a final plat, which was recorded as Partition Plat 2021-25.

**I. APPROVAL CRITERIA\_MORROW COUNTY ZONING ORDINANCE**  
 MCZO Criteria are shown below in bold highlight followed by response in standard print.

**1.050. ZONING PERMIT.**

**Prior to the construction, reconstruction, alteration, or change of use of any structure larger than 100 square feet or use for which a zoning permit is required, a zoning permit for such construction, reconstruction, alteration, or change of use or uses shall be obtained from the Planning Director or authorized agent thereof. A zoning permit shall become void after 1 year unless the development action has commenced. A 12-month extension may be granted when submitted to the Planning Department prior to the expiration of the approval period.**

Applicant is required to obtain a zoning permit for the proposed use pursuant to MCZO 3.070.A. below.

**3070.A. - Uses Permitted Outright.**

**In an M-G Zone, the following uses and their accessory uses are permitted outright; except as limited by subsection C of this section. A Zoning Permit is required for development and projects larger than 100 acres are subject to Site Development Review (Article 4 Supplementary Provisions Section 4.170 Site Development Review)**

**16. Data center**

As set forth above, MCZO 3.070.A., the data center use and its accessory uses are permitted outright in the MG zone. Section 3.070 requires Site Development Review pursuant to MCZO 4.170. However, Section 4.170 does not exist in the MCZO. Site Plan Review is required pursuant to MCZO 4.165 is a different process with different criteria.

**3.070.C. Use Limitations. In an M-G Zone, the following limitations and standards shall apply to all permitted uses:**

- 1. No use permitted under the provisions of this section that requires a lot area exceeding two (2) acres shall be permitted to locate adjacent to an existing residential lot in a duly platted subdivision, or a lot in a residential zone, except as approved by the Commission.**

The proposed data center campus, including accessory uses and buildings will require a lot area larger than two acres. Properties abutting the subject parcel to the north, south, east, and west of the subject property are zoned EFU. There is a residentially-zoned (FR2) lot and an existing residential subdivision located to the north and west of the subject property. Where the residential lots are adjacent to the subject property, the application may only be approved by the Planning Commission. The applicant presented five reasons why Planning Commission should approve the application and find the use compatible with the residential area.

1. The proposed use will comply with all applicable M-G development standards.

2. The primary data center use will be concentrated on the eastern portion of the subject property, which results in a buffer of a considerable distance (several acres) between these buildings and the residential lot.
3. The proposed use will utilize an access to and from Bombing Range Road, which is located on the eastern side of the subject property and not adjacent to residential lands. Trips related to the use will not typically travel by or near the residential area to the north.
4. The data center buildings may be designed to minimize adverse impacts on surrounding properties, such as noise, odor, dust, vibration, blasting, vapor, or bright lights.
5. The data center campus will be fenced and landscaped, which will provide screening.

In order to assure the data center development complies with the reasons presented by the applicant which are required to verify the use is compatible with the surrounding residential area, Planning Commission may include conditions of approval as listed below.

**2. No use permitted under the provisions of this section that is expected to generate more than 20 auto-truck trips during the busiest hour of the day to and from the subject property shall be permitted to locate on a lot adjacent to or across the street from a residential lot in a duly platted subdivision, or a lot in a residential zone.**

As illustrated on the site plan, the subject development will utilize a primary and a secondary access to Bombing Range Road. There are no residential lots in duly platted subdivisions or lots in residential zones along Bombing Range Road or adjacent to the subject property that are also accessed from Bombing Range Road. Therefore, trips to and from the development would not appear to adversely affect any surrounding areas planned or zoned for residential development. The Planning Commission may find the application complies with this standard.

**3070.D. - Dimension Requirements. The following Dimensional requirements apply to all buildings and structures constructed, placed or otherwise established in the MG zone.**

1. **Lot size and frontage: A minimum lot size has not been determined for this zone although the lot must be of a size necessary to accommodate the proposed use, however, it is anticipated that most, if not all uses will be sited on lots of at least two acres. The determination of lot size will be driven by the carrying capacity of the land given the proposed use. Minimum lot frontage shall be 300 feet on an arterial or collector; 200 feet on a local street.**

- a. Lot size: 126.92 acres
- b. Lot Frontage: 5,272.92 feet

2. **Setbacks:** No specific side or rear yard setbacks are identified within this zone but may be dictated by provisions of the Building Code or other siting requirements. The minimum setback between a structure and the right-of-way of an arterial shall be 50 feet. The minimum setback of a structure from the right-of-way of a collector shall be 30 feet, and from all lower-class streets the minimum setback shall be 20 feet. There shall be no setback requirement where a property abuts a railroad siding or spur if the siding or spur will be utilized by the permitted use.

a. Right of Way setback to nearest building: 927 feet

3. **Stream Setback:** All sewage disposal installations such as outhouses, septic tank and drain field systems shall be set back from the high-water line or mark along all streams and lakes a minimum of 100 feet, measured at right angles to the high-water line or mark. All structures, buildings, or similar permanent fixtures shall be set back from the high-water line or mark along all streams or lakes a minimum of 10 feet measured at right angles to the high-water line or mark.

a. No streams or lakes have been identified on site. Additionally, no wetlands are located on the site. See attached wetlands map.

4. **Uses adjacent to residential uses.** A sight-obscuring fence shall be installed to buffer uses permitted in the General Commercial Zone from residential uses. Additional landscaping or buffering such as diking, screening, landscaping or an evergreen hedge may be required as deemed necessary to preserve the values of nearby properties or to protect the aesthetic character of the neighborhood or vicinity.

a. A security fence is provided at the perimeter of the site with a dense picket arrangement. A chain link safety fence is provided at the perimeter of the industrial waste water retention pond. Additional landscape buffering will be provided as deemed necessary.

b. Lighting should be designed, installed and operated so as to minimize glare onto residential areas and general vicinity.

c. Noise from the generators and operation of the data center should be designed, developed and operated so as to minimize impacts to residential areas and the general vicinity.

These are also included as conditions of approval below.

### **3070.E. Transportation Impacts**

1. **Traffic Impact Analysis (TIA).** In addition to the other standards and conditions set forth in this section, a TIA will be required for all projects

**generating more than 400 passenger car equivalent trips per day. Heavy vehicles B trucks, recreational vehicles and buses B will be defined as 2.2 passenger car equivalents. A TIA will include: trips generated by the project, trip distribution for the project, identification of intersections for which the project adds 30 or more peak hour passenger car equivalent trips, and level of service assessment, impacts of the project, and, mitigation of the impacts. If the corridor is a State Highway, use ODOT standards. (MC-C-8-98)**

- a. According to the applicant, based on previous similar developments, the completed development is anticipated to generate less than 300 total trips per day. Therefore, a Traffic Impact Analysis may not be required. Applicant indicated that they will supplement traffic information in advance of the Planning Commission hearing. Although a TIA is not required, Morrow County Public Works may require signage and traffic control on Bombing Range Road during construction and during operations, in order to mitigate conflicts and to prevent accidents.

#### SECTION 4.010. ACCESS.

Intent and Purpose: The intent of this ordinance is to manage access to land development while preserving the flow of traffic in terms of safety, capacity, functional classification, and level of service.

Major roadways, including highways, arterials, and collectors serve as the primary network for moving people and goods. These transportation corridors also provide access to businesses and homes and have served as the focus for commercial and residential development. If access points are not properly designed, these roadways will be unable to accommodate the needs of development and retain their primary transportation function. This ordinance balances the right of reasonable access to private property with the right of the citizens of Morrow County and the State of Oregon to safe and efficient travel.

This ordinance shall apply to all public roadways under the jurisdiction of Morrow County and to application for development for any property that abuts these roadways.

This ordinance is adopted to implement the land access and access management policies of Morrow County as set forth in the Transportation System Plan. Access shall be provided based upon the requirements below:

- A. **Minimum Lot Frontage Requirement.** Every lot shall abut a street, other than an alley, for at least 50 feet, except on cul-de-sacs where the frontage may be reduced to 30 feet.

- a. Lot Frontage: 5,272.92 feet

- B. **Access Permit Requirement. Where access to or construction on a county road is needed, an access permit or right-of-way permit from Morrow County Public Works department is required subject to the requirements in this Ordinance. Where access to a state highway is needed, an access permit from ODOT is**



**required as part of the land use application. Where access is needed to a road managed by the Forest Service or other entity, an access permit or other authorization from the appropriate entity shall be required as part of the land use application.**

- a. Bombing Range Road is the proposed access. Bombing Range Road is identified as a county road. Applicant has obtained County approval for two access driveways on the subject property. See attached. The applicants contend that the primary access driveway is sufficiently deep to permit extensive on-site queueing which will alleviate the potential of queueing on Bombing Range Road. If circumstances change and queueing occurs on Bombing Range Road, applicant will be required to make off site improvements to guarantee safe operations of Bombing Range Road. Additionally, given that Bombing Range Road is not lighted and visibility is frequently limited during winter time conditions, especially foggy conditions, Public Works Director may recommend that the access points be lighted and signed. These are included as conditions of approval below.

- c. **Emergency Vehicle Access. It is the responsibility of the landowner to provide appropriate access for emergency vehicles at the time of development. A dead-end private street exceeding one hundred-fifty (150) feet in length shall have an adequate turn around facility approved by the appropriate Fire Marshal or, if the Fire Marshal fails to review the private street, approval by the Building Official or his designee.**

The proposed development has an internal loop road which is adequately sized to allow emergency vehicular ingress and egress from the development.

- II. **AGENCIES NOTIFIED:** Eric Imes, Morrow County Public Works Director; Anne Debbaut, DLCD Region Representative; State Fire Marshall; City of Boardman; City of Irrigon; Mike Gorman, County Assessor; Glenn McIntire, County Building Official; Justin Nelson, County Counsel; Lisa Mittelsdorf, Mark Patton, Jacob Cain, Port of Morrow; Kimberly Peacher, NAS Whidbey Island; Boardman Fire District, Tom Lapp, ODOT Permit Specialist, Pendleton; Teresa Penninger, ODOT Region 5 Planning Manager.

**III. LEGAL NOTICE PUBLISHED:**

February 1, 2022 East Oregonian  
February 2, 2022 Heppner Gazette

**IV. PROPERTY OWNERS NOTIFIED:** January 24, 2022

- V. **CONCLUSION:** Planning Commission may approve the Zoning Permit subject to conditions of approval.

**VI. CONDITIONS OF APPROVAL:**

1. Submit a lighting plan and glare analysis. Lighting should be designed, installed and operated so as to minimize glare onto residential areas and general vicinity
2. Comply with light mitigation recommendations of NAS Whidbey Island Air Station, if necessary, to mitigate impacts to flight operations at the US Naval Bombing Range.
3. Provide a landscape design plan to include at a minimum fencing, landscaping and lighting.
4. Comply with signage and lighting at access points as recommended by Morrow County Public Works.
5. If conditions warrant in the future, owner will be required to construct off-site improvements to Bombing Range Road in order to minimize traffic hazards.
6. Obtain land use permit for application of industrial wastewater on lands not located on the subject parcel.
7. Obtain land use permit for utility substation.
8. Obtain land use permit for new transmission lines.
9. Obtain access or right of way permit for new transmission line.
10. Construct a chain link around the perimeter of the industrial waste water retention pond.
11. Noise from the generators and operation of the data center should be designed, developed and operated so as to minimize impacts to residential areas and the general vicinity.

**DECISION OF THE PLANNING COMMISSION:**

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Jeff Wenholz, Chair  
Morrow County Planning Commission

Date

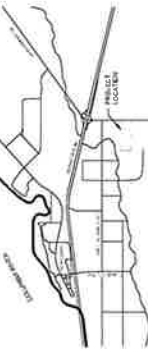
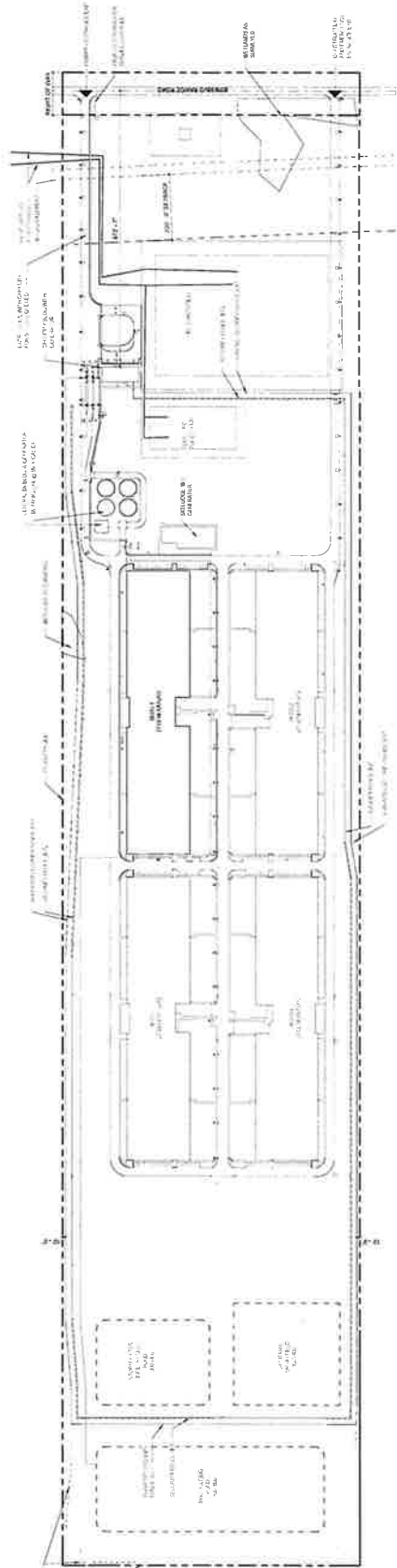
Attachments: Vicinity Map, Property Owner Map, Wetlands Map, Site Plan Layout, Access Permits

NOTES

CONFIDENTIAL

BOMBING RANGE ROAD,  
BONDIAN, OR 97116

PERMIT SET



GENERAL DIMENSIONAL STANDARDS

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1 ZONING ARCHITECTURAL SITE PLAN

# Vicinity Map



Scale  
0 1,500 3,000 Feet



**ZP-2956-22**  
AWS (Windy River)  
4N 25E TL 1701

### Legend

Tax Lot

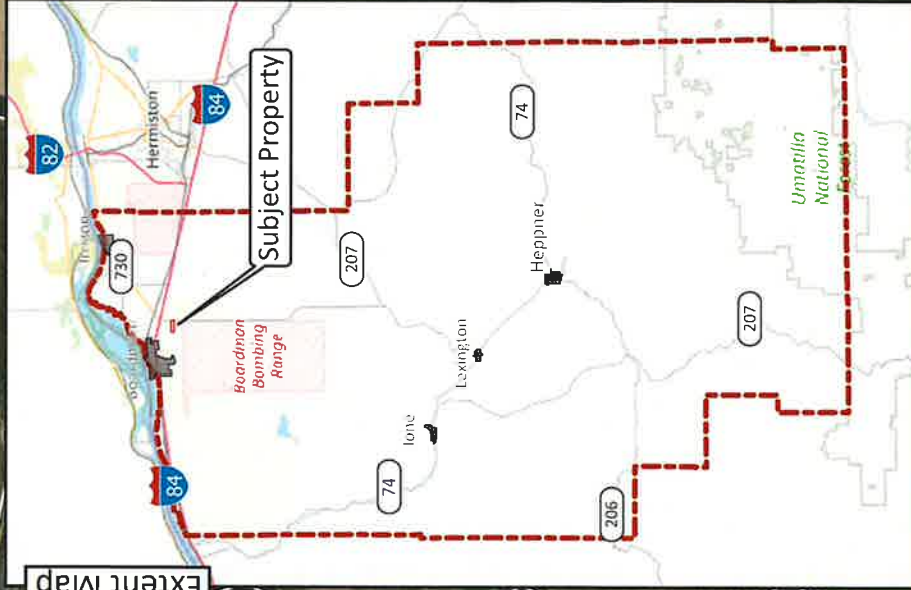


Subject Property



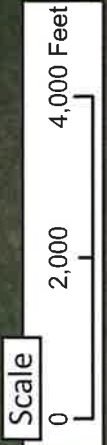
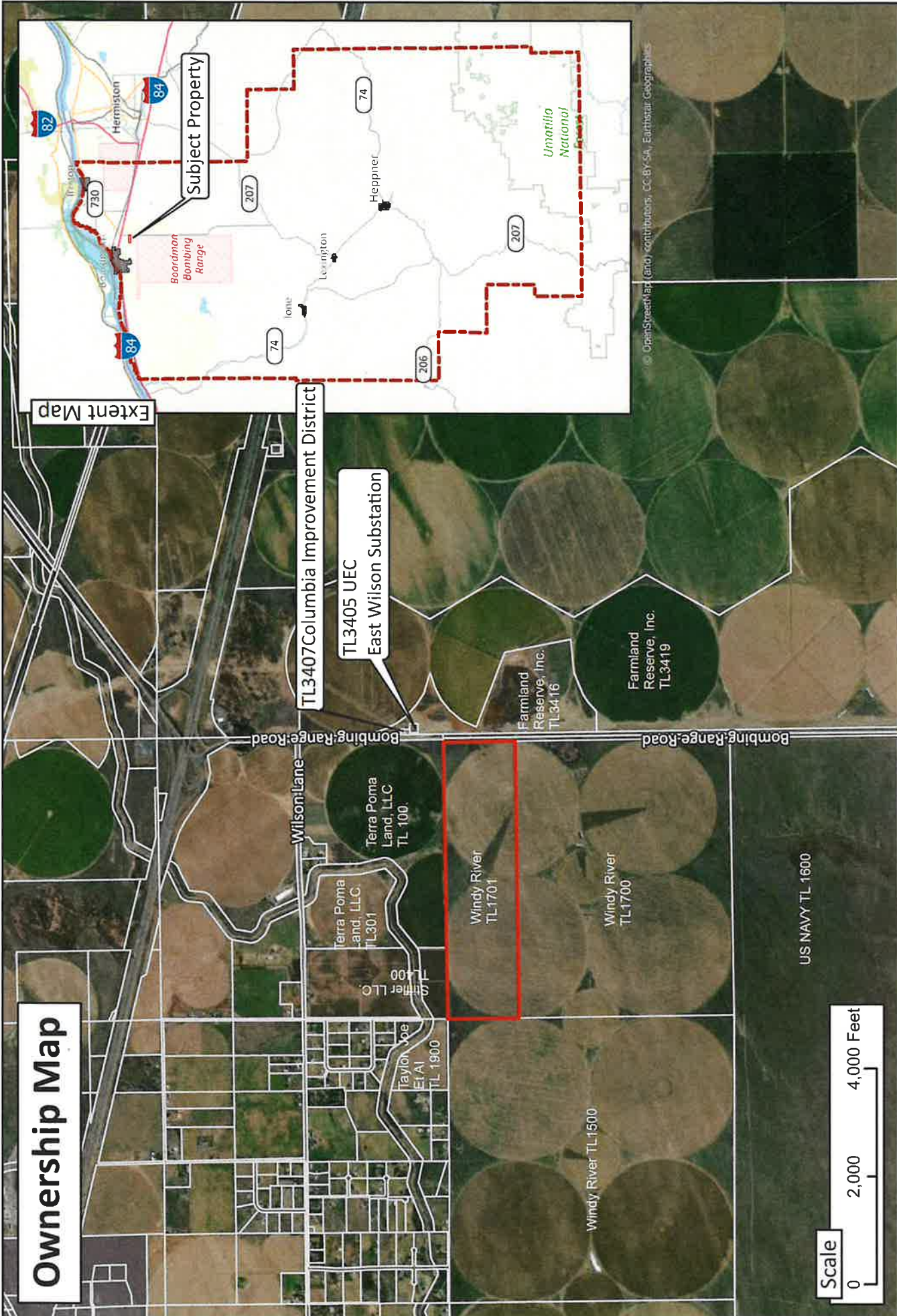
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Cartography By: Stephen Wrecks  
Morrow County Planning Department  
Coordinate System: NAD83 Oregon GIC Lambert ft  
Datum: North American 1983  
Projection: Lambert Conformal Conic  
Source: S:\Morrow\GIS\Production\7-2-2021\10-14-1983-21.Wrecks.mxd



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# Ownership Map



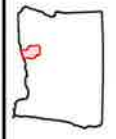
**ZP-2956-22**  
**AWS (Windy River)**  
**4N 25E TL 1701**

**Legend**

Tax Lot



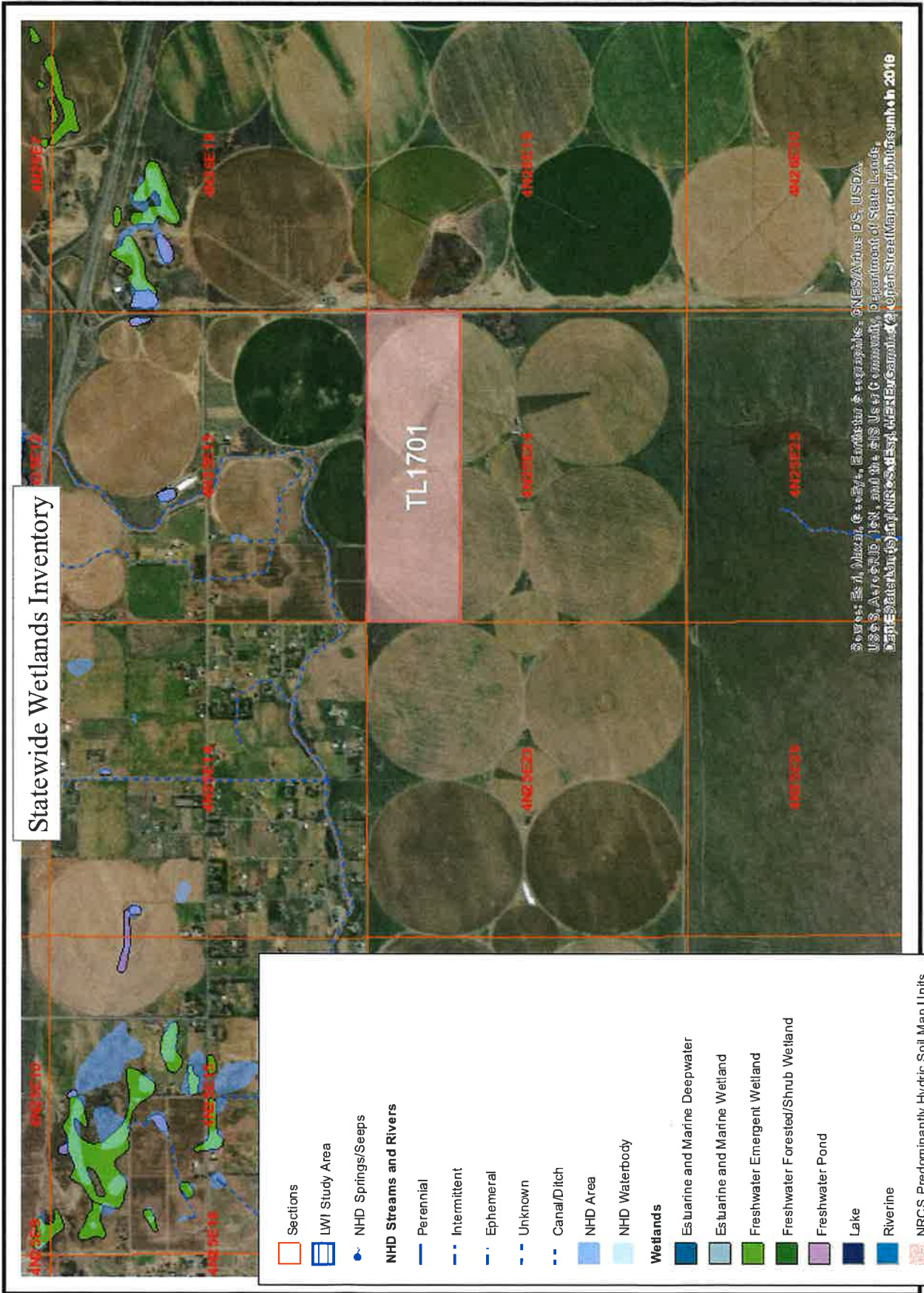
Subject Property



Date Saved: 2/9/2022 12:00 PM

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 Morrow County Planning Department  
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 Projection: Lambert Conformal Conic  
Order: 5 | Planning | Planning Forms & Maps | 02/22 | 2:56:52 | View | Print





# Statewide Wetlands Inventory

- Sections
- LWI Study Area
- NHD Springs/Seeps
- NHD Streams and Rivers**
  - Perennial
  - Intermittent
  - Ephemeral
  - Unknown
  - Canal/Ditch
  - NHD Area
  - NHD Waterbody
- Wetlands**
  - Estuarine and Marine Deepwater
  - Estuarine and Marine Wetland
  - Freshwater Emergent Wetland
  - Freshwater Forested/Shrub Wetland
  - Freshwater Pond
  - Lake
  - Riverine
  - NRCS Predominantly Hydric Soil Map Units
  - NRCS Agate-Winlo Soils in Jackson County

The Statewide Wetlands Inventory (SWI) represents the best data available at the time this map was published and is updated as more information becomes available. It does not represent the presence, absence and boundaries of wetlands and waters (such as creeks and ponds). Aerial photography is used to identify wetland conditions.

1 inch = 0.57 miles

0 0.25 0.45 0.9 1.35 1.8 miles

Date: 2/9/2022

State of Oregon  
 Department of State Lands  
 775 Summer Street, NE, Ste 100  
 Salem, OR 97301-1276  
 (503) 586-5200

Source: Esri, DigitalGlobe, GeoEye, Earthstar (United States), CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community  
 Department of State Lands  
 Data Statistics in ArcSWAT, ArcSWAT, ArcSWAT, ArcSWAT, ArcSWAT

<https://www.oregon.gov/dsl/www/Pages/SWI.aspx>



# PUBLIC WORKS DEPARTMENT

*Airport General Maintenance Road Department Parks Transfer Stations*

365 W. Highway 74  
 P.O. Box 428  
 Lexington, OR. 97839  
 Phone: (541) 989-9500  
 Fax: (541) 989-8352

Matt Scrivner  
 Director

Eric Imes  
 Asst. Road Master

Sandi Putman  
 Management Asst.

Kirsti Cason  
 Administrative Asst.

November 2, 2021

RE: Approach Permit # 288  
 Construction Permit # 288a

Parametrix  
 Attn: Scott Bolduc  
 150 NW Pacific Park Lane, Suite 110  
 Bend, Oregon 97702

To Whom it may concern,

Thank you for submitting a Approach Site Approval Application & Construction Permit. Your approach site application (#288) was Recommended and Approved on October 27, 2021. Included with this letter is a construction permit (#288a), which was approved on October 27, 2021. Please note that a Construction Permit will EXPIRE on January 27, 2022. Should you need an extension on your construction permit please contact the Morrow County Public Works Office.

County personnel may monitor progress of project during construction. Please contact the Morrow County Public Works office upon completing project so final inspection can be done.

Should you have any questions please feel free to contact us at 541-989-9500.

Thank you for your cooperation on this project.

Sincerely,

A handwritten signature in blue ink, appearing to read "Kirsti Cason".

Kirsti Cason  
 Administrative Assistant  
 Morrow County Public Works

Enclosures

cc: sbolduc@parametrix.com

Return to:  
**MORROW COUNTY PUBLIC WORKS**  
365 West Highway 74  
P.O. Box 428  
Lexington, Oregon 97839  
Phone: (541) 989-9500

**APPLICATION #:** 288  
**COUNTY ROAD #:** ~~ESA 829~~ 490  
**ROAD NAME:** Bombing Range Rd.

**Applicant Mailing Address**  
Parametrix, Attn: Scott Bolduc (agent)  
Name (Business Name, Attn: Name)  
150 NW Pacific Park Lane, Suite 110  
Mailing Address (Street/Post Office Box)  
Bend, OR 97702  
City, State, Zip Code  
541-508-7710  
Phone Number

**APPLICATION FEE:**  
(CHECK ONE)  
 Private (\$50.00)  Commercial (\$125.00)

**PAYMENT RECEIVED:**  
10/11/2021 - \$ 625.00  
(Date Payment Received - Amount Received - Initials)  
ck# 335

SBolduc@parametrix.com  
Phone Number

**APPLICATION FOR APPROACH SITE APPROVAL**  
(Approach Roads, Private Crossings, Utility Service Lines, Fixtures and Other Facilities)

*Please fill out this form completely in ink (Blue or Black) or type.*

We, Scott Bolduc/Parametrix (agent)  
(Name - Individual/Business) (Physical Address of approach)  
hereby respectfully request site approval either to locate within County Road right of way or  
cross Morrow County Road Bombing Range Road, at Pl.905 (N45-48-56-86 W119-37-24.58)  
(County Road Name) (County Road Name)  
24 4 North 25 E. W. M. with a Access Approach Permit for Private Driveway  
(Section) (Township) (Range) (Approach Road, Private Crossing, ect.)  
as more particularly described by the attached sketch, and/or deed reference.

This is only a **site approval**. A Construction Permit will need to be obtained at the time of construction. Approval to build will have to be issued by Morrow County Public Works **before construction may begin**.

**APPLICANT AGREES TO THE TERMS AND CONDITIONS ON THE ATTACHED FOUR ( 4 ) PAGES.** ( THE ATTACHED PAGES REQUIRE INITIALS OF APPLICANT )

**APPLICANT SIGNATURE:** *Scott Bolduc* **DATE:** 09-28-2021  
(Signature of Authorized Applicant) (Date Signed)

State of OREGON  
County of DESCHUTES  
This instrument was acknowledged before me on SEPTEMBER 28, 20 21  
by PAMELA L. HORNBERGER  
PAMELA L. HORNBERGER  
Notary Public - State of 7/28/23

OFFICIAL STAMP  
PAMELA LYNN HORNBERGER  
NOTARY PUBLIC - OREGON  
COMMISSION NO. 999458  
MY COMMISSION EXPIRES JULY 28, 2023

Denied permit application may be appealed to the Morrow County Board of Commissioners

**RECOMMENDED BY:** *Eric L. Lines* **DATE:** 10/27/2021  
(Assistant Road Master) (Date Approved)  
**APPROVED BY:** *[Signature]* **DATE:** 10/27/2021  
(Public Works Director) (Date Approved)

**ATTEST** \_\_\_\_\_  
(Morrow County Clerk)





## APPROACH ROAD CONSTRUCTION

### APPLICANT AGREES TO THE FOLLOWING TERMS AND CONDITIONS:

1) Approach road or crossing will, in all instances, enter the county road at right angles to the existing roadway unless otherwise authorized by the Public Works Department (hereafter referred to as "Department"). Refer to the attached "STANDARD DRAWINGS FOR ROAD APPROACH CONSTRUCTION", hereafter refer to as the STANDARD DRAWINGS.

2) Approach road or crossing shall be constructed to the lines, grades and dimensions of the STANDARD DRAWINGS. All materials shall meet the approval of the Department.

3) Compaction requirements are as follows:

In the event that native earth fill is required, it shall be placed in uniform lifts, not exceeding 12 inches in thickness, and each lift shall be thoroughly compacted.

3"-0 base rock shall be spread in two lifts and each lift thoroughly compacted.

3/4"-0 crushed aggregate shall be spread and thoroughly compacted to the finish lines and grade.

Asphalt Concrete Pavement, if required, shall be placed and compacted to finish lines and grades meeting the requirements of the STANDARD DRAWINGS.

All compaction methods and results shall meet the approval of the Department. In the event the Department believes sufficient compaction of any material is not being achieved, it may require compaction testing. All such compaction testing shall be at the sole expense of the Applicant.

If the Department requires compaction testing, it shall consist of the following:

**Fill (Embankment Compaction):** 90% of maximum density. Maximum density and optimum moisture shall be determined in accordance with AASHTO T99 or T180. In place density shall be determined in accordance with AASHTO T191, T205, or T238.

**3"-0 Base Rock:** Compacted under appropriate compaction equipment, with no visible deflection under the wheels or drums of the compaction equipment.

**3/4"-0 Crushed Aggregate Leveling Course:** Shall be compacted to 95% of maximum density. Maximum density and optimum moisture shall be determined in accordance with AASHTO T99, with correction for oversize by AASHTO T224. In place density shall be determined in accordance with AASHTO T191, T205, or T238.

**Asphalt Pavement:** Shall be compacted to 92% of theoretical maximum density (Rice Density) as determined by the asphalt concrete mix design, for the asphalt concrete used. In place density shall be determined by nuclear gauge.

4) In the event that a cut slope of the existing county road must be excavated, the design of the approach shall insure such cut slope is not destabilized or the remaining cut slope is not damaged in any way. The approach road subgrade shall be constructed at the minimum width and dimension as shown on the STANDARD DRAWINGS.

#248

MORROW COUNTY PUBLIC WORKS  
Application for Approach Site Approval

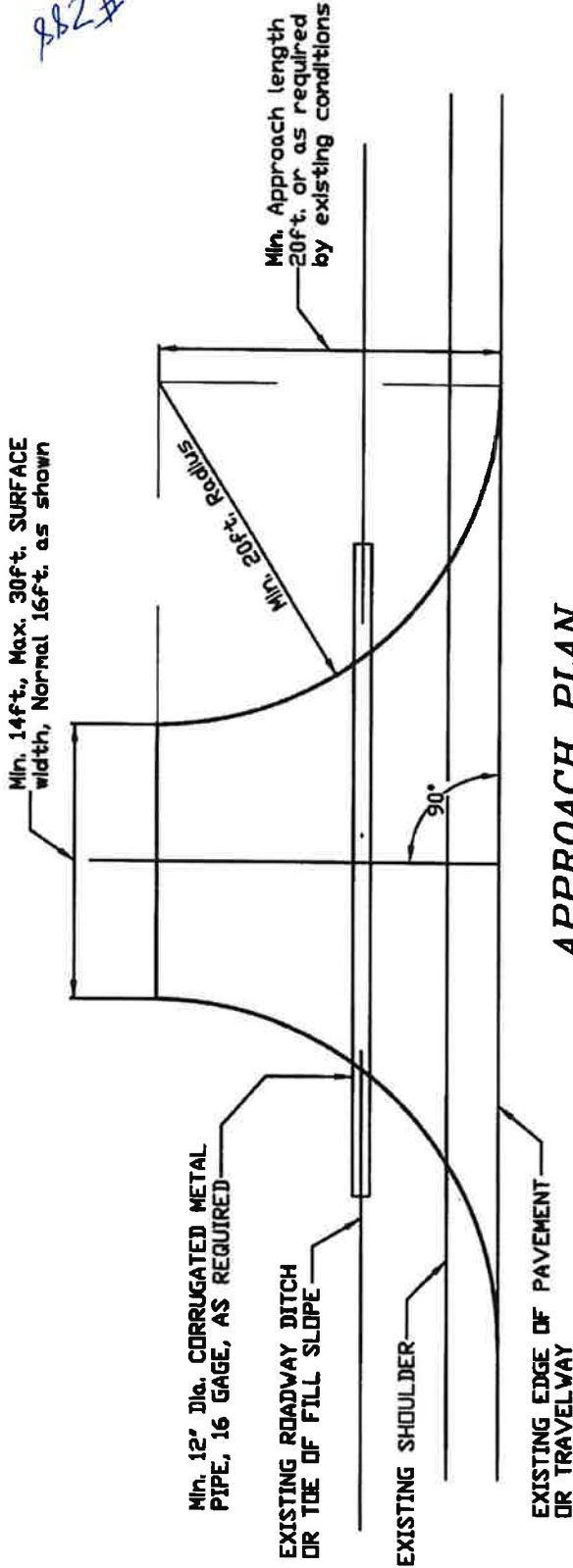
Page 1 SB  
(initial)

- 5) In the event a culvert pipe is required, it shall be installed so that the top of the pipe is below subgrade, and in no case less than 12" below the finish surface of the approach (asphalt thickness not included). Where practical, the culvert shall be installed in the natural drainage or previously constructed ditch along the existing county road. If the culvert flow line must be placed lower than the natural drainage or previously constructed ditch, inlet and outlet channels shall be constructed to insure the proper drainage flow and function of the culvert. The Department shall be consulted and an approved method for culvert installation shall be given before any culvert pipes are installed. Culvert shall be bedded and backfilled with ¾"-0 crushed aggregate. Refer to the STANDARD DRAWINGS for Culvert and Trench Details.
- 6) If Morrow County installs a culvert, approach or crossing, the cost of installation and construction will be tendered to Morrow County Public Works prior to issuance of the permit.
- 7) A minimum of at least one (1) week will be required to obtain Site Approval. This allows the Department time to review the application, make a site inspection and obtain the necessary signatures.
- 8) Applicant agrees to make this installation (if not performed by the Department), in a manner which will not in any way adversely affect the County Road which it is connected. Any damage to the County Road shall be the sole responsibility of the Applicant. The site shall be finished, cleaned and left in as good a condition as it was prior to construction. Any work required to finish or correct Approach Road construction shall be completed at no cost to Morrow County.
- 9) Applicant agrees to pay all costs incurred for the construction of the approach road or private road entrance, if installed by the County, at the applicant's request, as set forth by this application.
- 10) Applicant declares that they are the owner or lessee of real property abutting the above-described roadway and has the lawful authority to apply for this permit.
- 11) Applicant agrees to have all work completed, including final inspection and approval, within 90 days after the date of approval of this application. After 90 days, this application becomes null and void.
- 12) In addition to the requirements given herein and as shown on STANDARD DRAWINGS, all activities and construction shall be carried out in accordance with the standards given in Morrow County Code 8.08.030 and Code 8.08.040, and shall meet the approval of the Department.
- 13) Morrow County Code 8.08.060, pursuant to ORS 374.305, allows Morrow County, at their option, to remove from their right of way, any obstruction formed by improperly constructed approaches, and such cost will be charged to the violator (S6 of Ord. Dated 8/6/67).

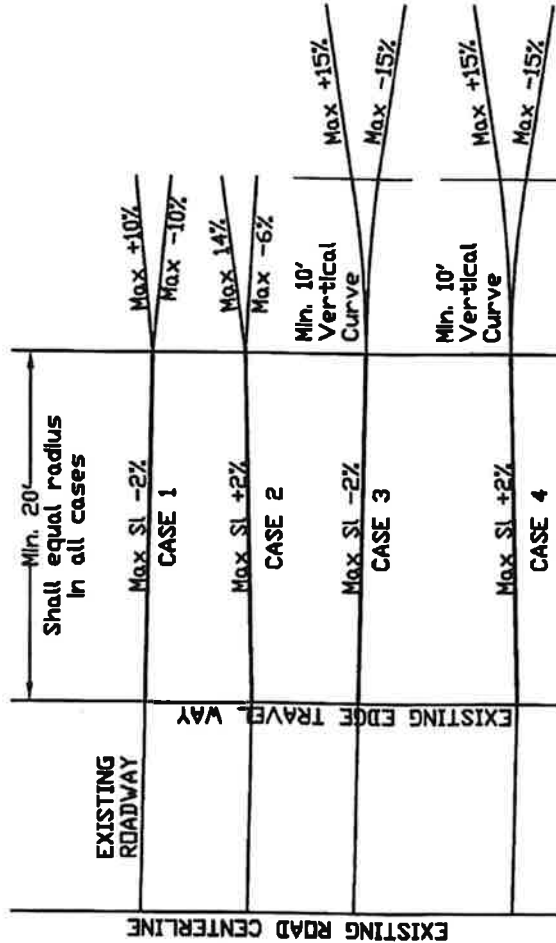
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MORROW COUNTY PUBLIC WORKS  
 Application for Approach Site Approval  
 Page 3 SB (Initial)

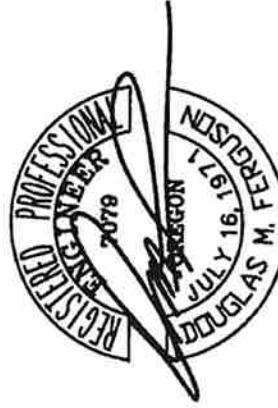
882#



**APPROACH PLAN**  
 Scale 1" = 10'



**APPROACH PROFILE DETAIL**  
 No Scale



RENEWAL DATE:  
 01/01/2012

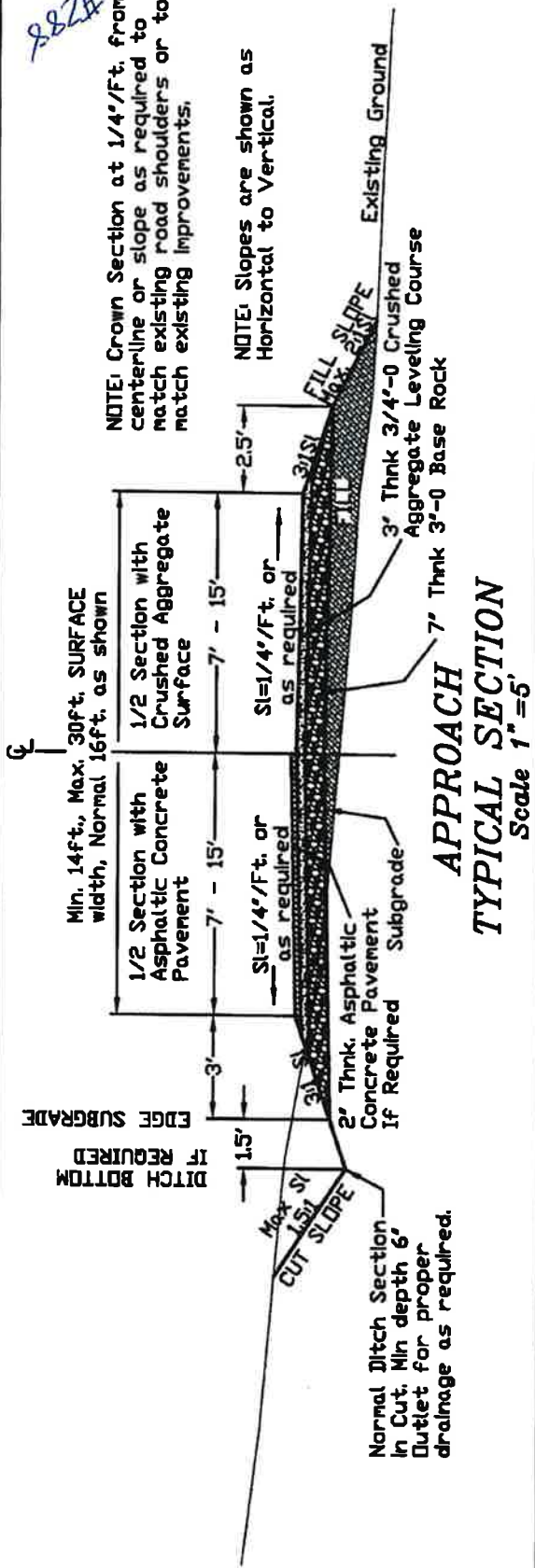
MORROW COUNTY  
 PUBLIC WORKS DEPARTMENT  
 STANDARD DRAWINGS FOR  
 ROAD APPROACH CONSTRUCTION  
 SHEET 1 OF 2

MORROW COUNTY PUBLIC WORKS  
 Application for Approach Site Approval  
 Page 4 (Initial)

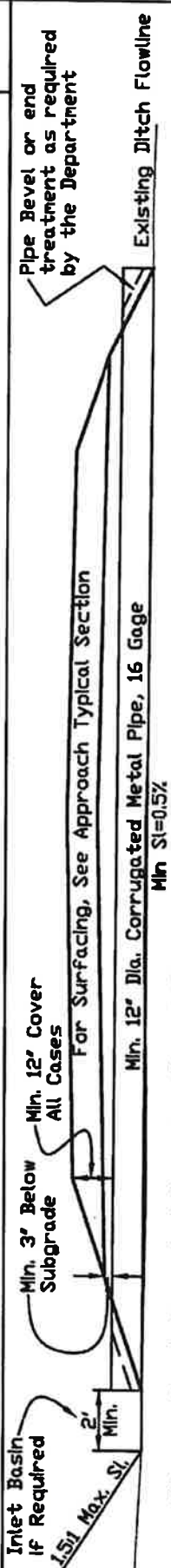
282#

NOTE: Crown Section at 1/4"/Ft. from centerline or slope as required to match existing road shoulders or to match existing improvements.

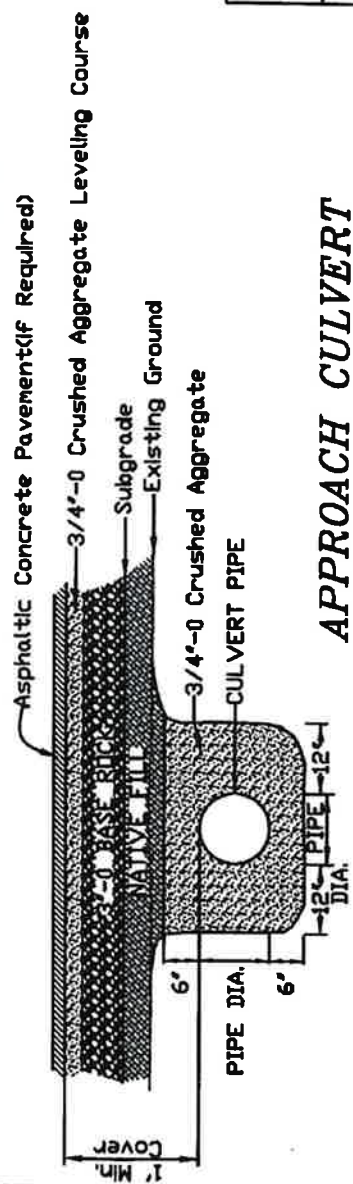
NOTE: Slopes are shown as Horizontal to Vertical.



**APPROACH TYPICAL SECTION**  
 Scale 1" = 5'



**APPROACH TRENCH DETAIL**  
 Scale 1" = 6'



**APPROACH TRENCH DETAIL**  
 No Scale



RENEWAL DATE:  
 01/01/2012

MORROW COUNTY  
 PUBLIC WORKS DEPARTMENT  
 STANDARD DRAWINGS FOR  
 ROAD APPROACH CONSTRUCTION  
 SHEET 2 OF 2

## Tamra Mabbott

---

**From:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA)  
<kimberly.n.peacher.civ@us.navy.mil>  
**Sent:** Monday, February 14, 2022 3:08 PM  
**To:** Tamra Mabbott  
**Cc:** Farmer, Todd E NFG NG ORARNG (USA)  
**Subject:** RE: Data Center

Hello Tamra,

I reviewed the conditions and details provided by the developer.

If this project does move forward we would like to see their lighting plan to see how they ensure their 'intent' to minimize lighting is planned.

Thank you.

V/R,

Kimberly Peacher  
Community Planning & Liaison Officer  
Northwest Training Range Complex  
(360) 930-4085

-----Original Message-----

**From:** Tamra Mabbott <tmabbott@co.morrow.or.us>  
**Sent:** Monday, February 14, 2022 9:33 AM  
**To:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA)  
<kimberly.n.peacher.civ@us.navy.mil>  
**Cc:** Farmer, Todd E NFG NG ORARNG (USA) <todd.e.farmer.nfg@army.mil>  
**Subject:** [URL Verdict: Neutral][Non-DoD Source] RE: Data Center

Hello Kim - I will share your comments with the Planning Commission. I think both questions will be answered if the Commission agrees to the conditions in the draft Findings.

Preliminary Findings for the data center begins on page 22 of the packet here:

[https://linkprotect.cudasvc.com/url?a=https%3a%2f%2fusg01.safelinks.protection.office365.us%2f%3furl%3dhttps%253A%252F%252Fwww.co.mo&c=E,1,Uoclqlu0cqbmGqC0CgUknhx9m7rihTO3b\\_gCkNWLbZtU-Fhd5S26avSEu05J6DuWq3XiyyUC-d5ft4GHrgo7eM\\_Fv4xfPV1ZLQ\\_r8tPZ3hWfsJKOm7bC1tFCSICd&typo=1rrow.or.us%2Fsites%2Fdefault%2Ffiles%2Ffileattachments%2Fplanning\\_commission%2Fmeeting%2Fpackets%2F16324%2F02222022\\_pc\\_packet\\_without\\_minutes.pdf&data=04%7C01%7Ckimberly.n.peacher.civ%40us.navy.mil%7C4be0d545ca604ad301ed08d9efe029f3%7Ce3333e00c8774b87b6ad45e942de1750%7C0%7C0%7C637804569654075917%7CUnknwn%7CTWFpbGZsb3d8eyJWljoimC4wLjAwMDAiLCJljoiv2luMzliLCJBTil6lk1haWwILCjXVCI6Mn0%3D%7C3000&data=cWORPjSgk947ipPOcb0oNdhYez7OW5DJObumEj%2FL2c%3D&reserved=0](https://linkprotect.cudasvc.com/url?a=https%3a%2f%2fusg01.safelinks.protection.office365.us%2f%3furl%3dhttps%253A%252F%252Fwww.co.mo&c=E,1,Uoclqlu0cqbmGqC0CgUknhx9m7rihTO3b_gCkNWLbZtU-Fhd5S26avSEu05J6DuWq3XiyyUC-d5ft4GHrgo7eM_Fv4xfPV1ZLQ_r8tPZ3hWfsJKOm7bC1tFCSICd&typo=1rrow.or.us%2Fsites%2Fdefault%2Ffiles%2Ffileattachments%2Fplanning_commission%2Fmeeting%2Fpackets%2F16324%2F02222022_pc_packet_without_minutes.pdf&data=04%7C01%7Ckimberly.n.peacher.civ%40us.navy.mil%7C4be0d545ca604ad301ed08d9efe029f3%7Ce3333e00c8774b87b6ad45e942de1750%7C0%7C0%7C637804569654075917%7CUnknwn%7CTWFpbGZsb3d8eyJWljoimC4wLjAwMDAiLCJljoiv2luMzliLCJBTil6lk1haWwILCjXVCI6Mn0%3D%7C3000&data=cWORPjSgk947ipPOcb0oNdhYez7OW5DJObumEj%2FL2c%3D&reserved=0)

## George Nairns

---

**From:** Tamra Mabbott  
**Sent:** Thursday, February 17, 2022 9:06 AM  
**To:** 'Kristen Tiede'  
**Cc:** George Nairns  
**Subject:** RE: Agency Notice ZP-2956-22

Good morning Kristen.

Thank you for your comments.

We will add this email to the record for the Planning Commission.

I will propose that the Planning Commission include your request as one of the conditions of approval.

Please feel free to participate via zoom or in person if you would like to participate or make further comment.

Cordially,

Tamra

**From:** Kristen Tiede <KristenTiede@ctuir.org>  
**Sent:** Thursday, February 17, 2022 8:31 AM  
**To:** Tamra Mabbott <tmabbott@co.morrow.or.us>  
**Subject:** FW: Agency Notice ZP-2956-22

**STOP and VERIFY - This message came from outside of Morrow County Government.**

Hello Tamra,

The Confederated Tribes of the Umatilla Indian Reservation (CTUIR) Cultural Resources Protection Program (CRPP) has reviewed the Zoning Permit Z-2956-22. As the project area has not been previously surveyed, the CRPP recommends an archaeological survey with subsurface testing prior to development. Please let me know if there are any questions or concerns.

Thank you,  
 Kristen

Kristen Tiede  
 Archaeologist  
 Cultural Resources Protection Program  
 Confederated Tribes of the Umatilla Indian Reservation  
 46411 Timine Way, Pendleton, OR 97801  
 Direct Line/Fax: (541) 429-7206  
 Main Office: (541) 276-3447  
[KristenTiede@ctuir.org](mailto:KristenTiede@ctuir.org)



**From:** George Nairns [<mailto:gnairns@co.morrow.or.us>]  
**Sent:** Tuesday, February 1, 2022 4:05 PM  
**To:** [anne.debbaut@state.or.us](mailto:anne.debbaut@state.or.us); [craig.anderson@osp.oregon.gov](mailto:craig.anderson@osp.oregon.gov); [teresa.b.penninger@odot.state.or.us](mailto:teresa.b.penninger@odot.state.or.us);

RECEIVED 27  
JAN 18 2022



LAND USE APPLICATION  
ZONING PERMIT

BY: [Signature]

PAID  
1-18-22  
PJP

File Number 2956 Date Received \_\_\_\_\_ Date Deemed Complete \_\_\_\_\_ Fee \$250

Applicant / Contractor: Name(s) Yost Grube Hall Architecture

Mailing Address 707 SW Washington Street, Suite 1200

Phone (503) 891 - 5091 E-mail address jessew@ygh.com

Legal Owner (if different from applicant):  
Name(s) Amazon Data Services, Inc.

Address 410 Terry Avenue North, Seattle, WA 98109

Property Description:

Township 4N Range 25E Section 24 Tax Lot 3416 Zoning Designation MG

Physical Address \_\_\_\_\_

Located within a UGB? No If yes, which city? \_\_\_\_\_ Legal Access Bombing Range Road

Subdivision/Partition Parcel 1, Partition Plat 2021- Lot Width 1045.68' ft Lot Depth 5272.92' ft

Size of Parcel 126.92 acres Size of Tract \_\_\_\_\_ acres

Proposed Set Backs: Front 30 ft Side 10 ft Side 10 ft Rear 10 ft

- Proposed Structures:
- 1. Data Hall - Building 1 Sq Ft 215,461 Bdrms \_\_\_\_\_ Baths 9 Toilets, 2 Urinals
  - 2. Security Bldg Sq Ft 6,560 Bdrms \_\_\_\_\_ Baths 3 Toilets
  - 3. Water Treatment Bldg Sq Ft 2,112 Bdrms \_\_\_\_\_ Baths 0 Toilets

Plot Plan: Attach a plot plan showing where on the lot the structures will be located. Identify set backs, existing structures, location of access, septic system, drainfield, and well if applicable. The drawing does not need to be to scale.

Certification: I, the undersigned, acknowledge that I am familiar with the standards and limitations set forth by the Morrow County Zoning and Subdivision Ordinance. I propose to meet all standards set forth by the County's Zoning and Subdivision Ordinance and any applicable State and Federal regulations. I certify that the statements and information provided with this application are true and correct to the best of my knowledge.

Signed: [Signature]  
(Applicant / Contractor)

Matthew Dixon  
(Legal Owner)

Printed: Jesse Wait  
(Applicant / Contractor)

Matthew Dixon, Authorized Signatory  
(Legal Owner)

If this application is not signed by the property owner, a letter authorizing signature by the applicant must be attached.

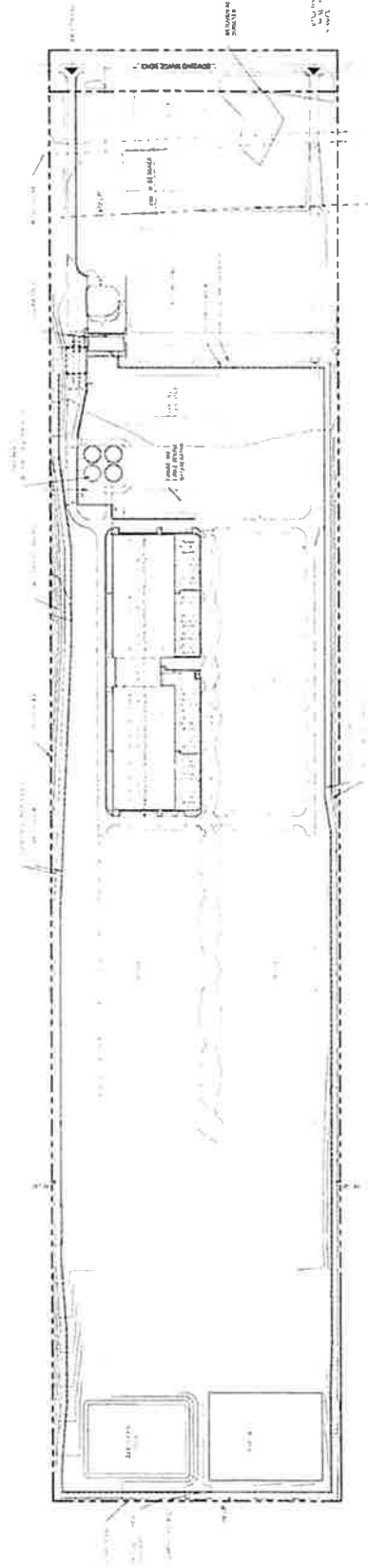
Planning Approval Signature \_\_\_\_\_ Date \_\_\_\_\_

Morrow County Planning Department  
P.O. Box 40, Irrigon Oregon 97844  
(541) 922-4624 FAX: (541) 922-3472

- Distribution:  Planning Department - Original  Assessor's Office - Copy  Building Department  
 Port of Morrow  Owner  Applicant  Building Official



NOTES



**CONFIDENTIAL**  
 PROJECT OFFICER  
 BOMBING RANGE ROAD,  
 BOARDMAN, OR 97818  
 PROJECT NUMBER: 10000000000000000000  
**PERMIT SET**

1 ZONING ARCHITECTURAL SITE PLAN

**SITE PLAN LEGEND**

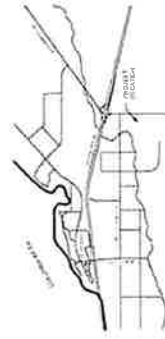
Symbol	Description
Circle with dot	PROPOSED BUILDING
Circle with cross	EXISTING BUILDING
Circle with slash	PROPOSED DRIVE
Circle with asterisk	EXISTING DRIVE
Circle with triangle	PROPOSED UTILITY
Circle with square	EXISTING UTILITY
Circle with diamond	PROPOSED TREE
Circle with circle	EXISTING TREE

**GENERATOR COUNTY:**

Area	Value
ACRES	10.00
FEET	1000
INCHES	12
MILES	0.01
YARDS	100

**GENERAL INDUSTRIAL (M-3) ZONE DIMENSIONAL STANDARDS**

MINIMUM	MAXIMUM
LOT AREA	10,000 SQ. FT.
MINIMUM LOT WIDTH	30 FT.
MINIMUM LOT DEPTH	30 FT.
MINIMUM FRONT SETBACK	10 FT.
MINIMUM SIDE SETBACK	10 FT.
MINIMUM REAR SETBACK	10 FT.
MAXIMUM BUILDING FOOTPRINT AREA	50% OF LOT AREA
MAXIMUM BUILDING HEIGHT	35 FT.
MAXIMUM NUMBER OF STORIES	3
MAXIMUM NUMBER OF UNITS	10
MAXIMUM NUMBER OF EMPLOYEES	100
MAXIMUM NUMBER OF TRUCKS	10
MAXIMUM NUMBER OF TRUCKS PER HOUR	10
MAXIMUM NUMBER OF TRUCKS PER DAY	100
MAXIMUM NUMBER OF TRUCKS PER WEEK	1000
MAXIMUM NUMBER OF TRUCKS PER MONTH	10000
MAXIMUM NUMBER OF TRUCKS PER YEAR	100000



VICINITY MAP

PROJECT INFORMATION  
 PROJECT NUMBER: 10000000000000000000  
 PROJECT NAME: BOMBING RANGE ROAD, BOARDMAN, OR 97818  
 PROJECT OFFICER: [Name]  
 PROJECT DATE: [Date]  
 PROJECT STATUS: [Status]

REVISIONS  
 NO. DESCRIPTION DATE BY

APPROVALS  
 PROJECT OFFICER: [Signature]  
 ZONING OFFICER: [Signature]

DATE: [Date]

PROJECT NUMBER: 10000000000000000000  
 PROJECT NAME: BOMBING RANGE ROAD, BOARDMAN, OR 97818  
 PROJECT OFFICER: [Name]  
 PROJECT DATE: [Date]  
 PROJECT STATUS: [Status]

REVISIONS  
 NO. DESCRIPTION DATE BY

APPROVALS  
 PROJECT OFFICER: [Signature]  
 ZONING OFFICER: [Signature]

DATE: [Date]

## SITE PLAN REVIEW NARRATIVE

### Proposed Development:

Scope of the proposed development includes a new 1 story, 215,461 square foot data center building, 2,116 square foot industrial water treatment building, and 6,560 square foot security building. Site work includes grading to accommodate a storm water pond for on-site storm water management, septic system for onsite wastewater treatment, drive aisles and parking areas, and generator yard pads.

### Future Development:

(3) Future Data Centers (similar in size and height to the proposed building), an electrical substation (dedicated power source for the data centers), associated parking and internal roadway improvements.

### Legal Description:

Parcel 1, Partition 2021-25, Township 4 North, Range 25 East, Section 24, Morrow County, Oregon. Tax lot 1701

### Previous Land Use Approval:

On August 3, 2021, after a noticed public hearing, the Planning Commission determined that, pursuant to ORS 197.713, the subject site is eligible for industrial development in buildings of any size and type, including accessory uses and on-site sewer facilities to serve same. See County File LUD-N-38-21.

## MORROW COUNTY ZONING ORDINANCE

### 1.050. ZONING PERMIT.

Prior to the construction, reconstruction, alteration, or change of use of any structure larger than 100 square feet or use for which a zoning permit is required, a zoning permit for such construction, reconstruction, alteration, or change of use or uses shall be obtained from the Planning Director or authorized agent thereof. A zoning permit shall become void after 1 year unless the development action has commenced. A 12-month extension may be granted when submitted to the Planning Department prior to the expiration of the approval period.

***Applicant is required to obtain a zoning permit for the proposed use pursuant to MCZO 3.070.A. below.***

Zoning:	M-G General Industrial
---------	------------------------

### 3070.A. - Uses Permitted Outright.

In an M-G Zone, the following uses and their accessory uses are permitted outright; except as limited by subsection C of this section. A Zoning Permit is required and projects larger than 100 acres are subject to Site Development Review (Article 4 Supplementary Provisions Section 4.170 Site Development Review)

#### **16. Data center**

***This section calls for Site Development Review pursuant to MCZO 4.170, which is not a valid cross-reference because this section does not exist in the MCZO. Moreover, applicant understands that Site Plan Review pursuant to MCZO 4.165 is a different process with different criteria. Accordingly, the MCZO does not identify specific criteria for this application. Below, applicant explains how the proposed use and development of the property is consistent with the use and development standards of the M-G zone. Based upon this explanation and the enclosed site plan, the County should issue the requested zoning permit.***

3.070.C. Use Limitations. In an M-G Zone, the following limitations and standards shall apply to all permitted uses:

1. No use permitted under the provisions of this section that requires a lot area exceeding two (2) acres shall be permitted to locate adjacent to an existing residential lot in a duly platted subdivision, or a lot in a residential zone, except as approved by the Commission.

***The proposed data center campus, including accessory uses and buildings, is permitted under the provisions of this section and will require a lot area larger than two acres. The properties to the north, south, east, and west of the subject property are zoned EFU. There is a residentially-zoned (FR2) lot diagonal to the subject property to the northwest. To the extent this lot is adjacent to the subject property, the Planning Commission should find that the proposed use is approved to develop in this location as proposed for the following five reasons. First, as explained in this narrative, the proposed use will comply with all applicable M-G development standards. Second, as illustrated in the site plan, the proposed use will be located on the eastern portion of the subject property, which results in a buffer of a considerable distance (several acres) from the residential lot. Third, the proposed use will take access only to/from Bombing Range Road, which is located on the eastern side of the subject property, which is on the opposite side from the residential lot. As a result, trips related to the use will not travel by or near the residential lot or the residential subdivision to its north. Fourth, the data centers and electrical substation will not generate adverse impacts on surrounding properties, such as noise, odor, dust, vibration, blasting, vapor, or bright lights. Fifth, the data center campus will be fenced and landscaped, which will provide screening. For all of these reasons, the County should find that the proposed use is compatible with residential uses in the surrounding area and should be approved. The Planning Commission should approve the use as proposed.***

2. No use permitted under the provisions of this section that is expected to generate more than 20 auto-truck trips during the busiest hour of the day to and from the subject property shall be permitted to locate on a lot adjacent to or across the street from a residential lot in a duly platted subdivision, or a lot in a residential zone.

***As illustrated on the site plan, the subject development will take access only from Bombing Range Road. There are no residential lots in duly platted subdivisions or lots in residential zones across Bombing Range Road or adjacent to the subject property that are also accessed from Bombing Range Road. Therefore, trips to and from the development will not pass by or adversely affect any surrounding areas planned or zoned for residential development. The Planning Commission should find this provision is not applicable.***

3070.D. - Dimension Requirements. The following Dimensional requirements apply to all buildings and structures constructed, placed or otherwise established in the MG zone.

1. Lot size and frontage: A minimum lot size has not been determined for this zone although the lot must be of a size necessary to accommodate the proposed use, however, it is anticipated that most, if not all uses will be sited on lots of at least two acres. The determination of lot size will be driven by the carrying capacity of the land given the proposed use. Minimum lot frontage shall be 300 feet on an arterial or collector; 200 feet on a local street.
  - a. **Lot size: 126.92 acres**
  - b. **Lot Frontage: 5,272.92 feet**
  
2. Setbacks: No specific side or rear yard setbacks are identified within this zone but may be dictated by provisions of the Building Code or other siting requirements. The minimum setback between a structure and the right-of-way of an arterial shall be 50 feet. The minimum setback of a structure from the right-of-way of a collector shall be 30 feet, and from all lower-class streets the minimum setback shall be 20 feet. There shall be no setback requirement where a property abuts a railroad siding or spur if the siding or spur will be utilized by the permitted use.
  - a. **Right of Way setback to nearest building: 927 feet**
  
3. Stream Setback: All sewage disposal installations such as outhouses, septic tank and drain field systems shall be set back from the high-water line or mark along all streams and lakes a minimum of 100 feet, measured at right angles to the high-water line or mark. All structures, buildings, or similar permanent fixtures shall be set back from the high-water line or mark along all streams or lakes a minimum of 10 feet measured at right angles to the high-water line or mark.
  - a. **No streams or lakes have been identified on site.**
  
4. Uses adjacent to residential uses. A sight-obscuring fence shall be installed to buffer uses permitted in the General Commercial Zone from residential uses. Additional landscaping or buffering such as diking, screening, landscaping or an evergreen hedge may be required as deemed necessary to preserve the values of nearby properties or to protect the aesthetic character of the neighborhood or vicinity.
  - a. **A security fence is provided at the perimeter of the site with a dense picket arrangement. Additional landscape buffering will be provided as deemed necessary.**

3070.E. Transportation Impacts

1. Traffic Impact Analysis (TIA). In addition to the other standards and conditions set forth in this section, a TIA will be required for all projects generating more than 400 passenger car equivalent trips per day. Heavy vehicles B trucks, recreational vehicles and buses B will be defined as 2.2 passenger car equivalents. A TIA will include: trips generated by the project, trip distribution for the project, identification of intersections for which the project adds 30

or more peak hour passenger car equivalent trips, and level of service assessment, impacts of the project, and, mitigation of the impacts. If the corridor is a State Highway, use ODOT standards. (MC-C-8-98)

- a. Based on previous similar developments, the completed development is anticipated to generate less than 300 total trips per day.***

#### SECTION 4.010. ACCESS.

**Intent and Purpose:** The intent of this ordinance is to manage access to land development while preserving the flow of traffic in terms of safety, capacity, functional classification, and level of service.

Major roadways, including highways, arterials, and collectors serve as the primary network for moving people and goods. These transportation corridors also provide access to businesses and homes and have served as the focus for commercial and residential development. If access points are not properly designed, these roadways will be unable to accommodate the needs of development and retain their primary transportation function. This ordinance balances the right of reasonable access to private property with the right of the citizens of Morrow County and the State of Oregon to safe and efficient travel.

This ordinance shall apply to all public roadways under the jurisdiction of Morrow County and to application for development for any property that abuts these roadways.

This ordinance is adopted to implement the land access and access management policies of Morrow County as set forth in the Transportation System Plan. Access shall be provided based upon the requirements below:

- A. **Minimum Lot Frontage Requirement.** Every lot shall abut a street, other than an alley, for at least 50 feet, except on cul-de-sacs where the frontage may be reduced to 30 feet.

- a. Lot Frontage: 5,272.92 feet***

- B. **Access Permit Requirement.** Where access to or construction on a county road is needed, an access permit or right-of-way permit from Morrow County Public Works department is required subject to the requirements in this Ordinance. Where access to a state highway is needed, an access permit from ODOT is required as part of the land use application. Where access is needed to a road managed by the Forest Service or other entity, an access permit or other authorization from the appropriate entity shall be required as part of the land use application.

- a. Access to Bombing Range Road is required. Bombing Range Road is identified as a county road. Applicant has obtained County approval of two access driveways on the subject property. See enclosed.***

- C. **Emergency Vehicle Access.** It is the responsibility of the landowner to provide appropriate access for emergency vehicles at the time of development. A dead-end private street exceeding

one hundred-fifty (150) feet in length shall have an adequate turn around facility approved by the appropriate Fire Marshal or, if the Fire Marshal fails to review the private street, approval by the Building Official or his designee.

- a. The proposed development has an internal loop road which is adequately sized to allow emergency vehicular ingress and egress from the development.***

**From:** Jesse Walt <[jessew@ygh.com](mailto:jessew@ygh.com)>  
**Sent:** Monday, January 24, 2022 3:05 PM  
**To:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>; Jeff Hope <[jeffh@ygh.com](mailto:jeffh@ygh.com)>  
**Cc:** Stephen Wrecsics <[swrecsics@co.morrow.or.us](mailto:swrecsics@co.morrow.or.us)>  
**Subject:** RE: Bombing Range Road zoning application - ADS

**STOP and VERIFY - This message came from outside of Morrow County Government.**

Hi Tamra,

We're happy to have a MS Teams call with you Thursday 1/27 at 1:30. Would you like YGH to set the call up?

Responses I can give you right now are as follows:

1. Power supply and source and transmission lines. Where are they located both on-site and off-site.
  - a. Power supply to buildings onsite will come from the proposed UEC substation, which will be designed and permitted under a separate project by the UEC. Offsite power access to the substation will be part of this project
2. ZP application proposes a data hall – Building 1, security building and water treatment building. Should it also include the substation?
  - a. The substation is a separate project by the UEC. We are not requesting any approvals for the substation as part of this application.
3. Traffic count – the application mentions ADT is 300 or less. Can you share your assumptions for both traffic during and post construction.
  - a. I can provide detailed traffic information for fully constructed campuses of similar size & usage in the region. I do not have access to traffic information for during construction but may be able to get an estimate if required for approval.
4. Site plan – does not show access locations onto Bombing Range Road or internal circulation/roads.
  - a. Far right side (East) of site plan indicates Main Entrance/Construction Entrance with notes & arrows. Internal circulation roads are indicated, but we can discuss on call if they need to be made graphically clearer.
5. Wetlands – the site plan references an “as surveyed” wetland. Do you have the supporting documentation?
  - a. Yes, this can be provided if needed.
6. What is the source of cooling water?
  - a. My understanding is Cooling water is being supplied by the Port of Morrow. YGH will confirm prior to Thursday’s call.
7. What is the plan for wastewater disposal?
  - a. Onsite septic treatment is being provided. The septic drain field is noted as “Drain Field” on the far left (West) side of the plan.

Application for zoning submittal. Please let us know if you need anything else from us (besides payment) for application. I will follow up this afternoon to confirm fee total and will pay online.

Also, It looks like the approach permit expires in 1/27. Is that something I can work with you to get extended or does that need to go through original applicant.

Thanks for you time.

Jeff Hope, AIA | Project Architect

Yost Grube Hall Architecture

707 SW Washington Street, Suite 1200 | Portland, OR 97205 | USA

t 503 221 0150 | d 503 715 3249 | w [ygh.com](http://ygh.com)







### LAND USE APPLICATION ZONING PERMIT

**RECEIVED**  
FEB 7 2022  
BY:    *JD*   

File Number 2956 Date Received \_\_\_\_\_ Date Deemed Complete \_\_\_\_\_ Fee \$500

Applicant / Contractor: Name(s) Yost Grube Hall Architecture

Mailing Address 707 SW Washington Street, Suite 1200

Phone (503) 891 - 5091 E-mail address jessew@ygh.com

Legal Owner (if different from applicant):

Name(s) Amazon Data Services, Inc.

Address 410 Terry Avenue North, Seattle, WA 98109

**Property Description:**

Township 4N Range 25E Section 24 Tax Lot 1701 Zoning Designation MG

Physical Address \_\_\_\_\_

Located within a UGB? No If yes, which city? \_\_\_\_\_ Legal Access Bombing Range Road

Subdivision/Partition Parcel 1, Partition Plat 2021-25 Lot Width 1045.68' ft Lot Depth 5272.92' ft

Size of Parcel 126.92 acres Size of Tract \_\_\_\_\_ acres

Proposed Set Backs: Front 972' 7" ft Side 10 ft Side 10 ft Rear 10 ft

- Proposed Structures:
- 1. Data Center Campus w/ associated support structures Sq Ft \_\_\_\_\_ Bdrms \_\_\_\_\_ Baths \_\_\_\_\_
  - 2. \_\_\_\_\_ Sq Ft \_\_\_\_\_ Bdrms \_\_\_\_\_ Baths \_\_\_\_\_
  - 3. \_\_\_\_\_ Sq Ft \_\_\_\_\_ Bdrms \_\_\_\_\_ Baths \_\_\_\_\_

**Plot Plan:** Attach a plot plan showing where on the lot the structures will be located. Identify set backs, existing structures, location of access, septic system, drainfield, and well if applicable. The drawing does not need to be to scale.

**Certification:** I, the undersigned, acknowledge that I am familiar with the standards and limitations set forth by the Morrow County Zoning and Subdivision Ordinance. I propose to meet all standards set forth by the County's Zoning and Subdivision Ordinance and any applicable State and Federal regulations. I certify that the statements and information provided with this application are true and correct to the best of my knowledge.

Signed:    *Jesse Walt*     
(Applicant / Contractor)

Digitally Signed by  
   *Matthew Dixon*     
DN: cn=Matthew Dixon, ou=  
(Legal Owner)

Printed: Jesse Walt  
(Applicant / Contractor)

Matthew Dixon, Authorized Signatory  
(Legal Owner)

If this application is not signed by the property owner, a letter authorizing signature by the applicant must be attached.

Planning Approval Signature \_\_\_\_\_ Date \_\_\_\_\_

Morrow County Planning Department  
P.O. Box 40, Irrigon Oregon 97844  
(541) 922-4624 FAX: (541) 922-3472

- Distribution:  Planning Department - Original  Assessor's Office - Copy  Building Department  
 Port of Morrow  Owner  Applicant  Building Official

## **SITE PLAN REVIEW NARRATIVE**

This application requests zoning permit approval, including a determination of consistency with Morrow County Zoning Ordinance (MCZO) 3.070, for a campus master plan, including the type and general location of all proposed primary and accessory uses/buildings. After obtaining approval of this application, the applicant will, as the site is built out in the future, obtain staff review and approval of detailed, building-specific zoning permits.

### **Proposed Development:**

As illustrated on the enclosed site plan, the scope of the proposed master plan development includes four new single story, 215,461 square foot data center buildings, 2,116 square foot industrial water treatment building and associated 490,000 gallon water storage tanks, 6,560 square foot security building, and 14,000 square foot data storage building. Site work includes grading to accommodate a storm water pond for on-site storm water management, septic system for onsite wastewater treatment, drive aisles and parking areas, and generator yard pads. Additionally, the Port of Morrow will be constructing a 5-acre Industrial Waste Water holding pond on the site. This pond will be part of a closed system, only accepting water from the data center campus and holding it until it can be used to irrigate surrounding agricultural uses.

The Umatilla Electric Cooperative (UEC) will provide power to the site from an on-site substation illustrated on the enclosed master site plan. As the dedicated power source for the data center, the substation is an accessory use. According to UEC, the utility currently owns and operates 230kV transmission facilities on the East side of Bombing Range Road. To serve this campus, UEC is proposing that upgrades be made to those existing facilities including a line extension starting near the UEC East Wilson Substation across Bombing Range Road to the west and south of the campus. The proposed line extension, which is not part of this application, is less than 0.5 miles in length and will be designed to minimize impact to private property and EFU. The UEC will obtain any additional right of way from private property owners, Morrow County Zoning Permits, and road crossing permit as required.

Domestic water service for site potable, construction, fire suppression, and industrial cooling will be delivered from the Port of Morrow (POM) east beach water system that is supplied by a combination of municipal water rights from alluvial, basalt, and surface water sources. The POM has the capacity to provide the service. Most of the 20" potable pipeline will be installed on POM owned property with a couple small sections of needed private easements. POM will be responsible for acquiring the private easements and has already begun that process in anticipation of this campus project. Additionally, POM will be responsible for obtaining any required crossing permits from ODOT, UPRR, and Morrow County.

The proposed site is located approximately 4,750 feet south of the interchange with Interstate 84/Highway 730.

### **Legal Description:**

Parcel 1, Partition Plat 2021-25, Township 4 North, Range 25 East, Section 24, Morrow County, Oregon.  
Tax lot 1701

### **Previous Land Use Approval:**

On August 3, 2021, after a noticed public hearing, the Planning Commission determined that, pursuant to ORS 197.713, the subject site is eligible for industrial development in buildings of any size and type, including accessory uses and on-site sewer facilities to serve same. See County File LUD-N-38-21.

exceeding two (2) acres shall be permitted to locate adjacent to an existing residential lot in a duly platted subdivision, or a lot in a residential zone, except as approved by the Commission.

***The proposed data center campus, including accessory uses and buildings, is permitted under the provisions of this section and will require a lot area larger than two acres. The properties to the north, south, east, and west of the subject property are zoned EFU. There is a residentially-zoned (FR2) lot diagonal to the subject property to the northwest. To the extent this single lot is adjacent to the subject property, the Planning Commission should find that the proposed use is approved to develop in this location as proposed for the following five reasons. First, as explained in this narrative, the proposed use will comply with all applicable M-G development standards. Second, as illustrated in the site plan, the primary data center use and related facilities will be concentrated on the eastern portion of the subject property, which results in a buffer of a considerable distance (several acres) between these buildings and the residential lot. Third, the proposed use will take access only to/from Bombing Range Road, which is located on the eastern side of the subject property, which is on the opposite side from the residential lot. As a result, trips related to the use will not travel by or near the residential lot or the residential subdivision to its north. Fourth, the data centers and electrical substation will not generate adverse impacts on surrounding properties, such as noise, odor, dust, vibration, blasting, vapor, or bright lights. Fifth, the data center campus will be fenced and landscaped, which will provide screening. For all of these reasons, the County should find that the proposed use is compatible with residential uses in the surrounding area and should be approved. The Planning Commission should approve the use as proposed.***

2. No use permitted under the provisions of this section that is expected to generate more than 20 auto-truck trips during the busiest hour of the day to and from the subject property shall be permitted to locate on a lot adjacent to or across the street from a residential lot in a duly platted subdivision, or a lot in a residential zone.

***As illustrated on the site plan, the subject development will take access only from Bombing Range Road. There are no residential lots in duly platted subdivisions or lots in residential zones across Bombing Range Road or adjacent to the subject property that are also accessed from Bombing Range Road. Therefore, trips to and from the development will not pass by or adversely affect any surrounding areas planned or zoned for residential development. The Planning Commission should find this provision is not applicable.***

3070.D. - Dimension Requirements. The following Dimensional requirements apply to all buildings and structures constructed, placed or otherwise established in the MG zone.

1. Lot size and frontage: A minimum lot size has not been determined for this zone although the lot must be of a size necessary to accommodate the proposed use, however, it is anticipated that most, if not all uses will be sited on lots of at least two acres. The determination of lot size will be driven by the carrying capacity of the land given the proposed use. Minimum lot frontage shall be 300 feet on an arterial or collector; 200 feet on a local street.

- a. Lot size: 126.92 acres
- b. Lot Frontage: 5,272.92 feet

#### SECTION 4.010. ACCESS.

Intent and Purpose: The intent of this ordinance is to manage access to land development while preserving the flow of traffic in terms of safety, capacity, functional classification, and level of service.

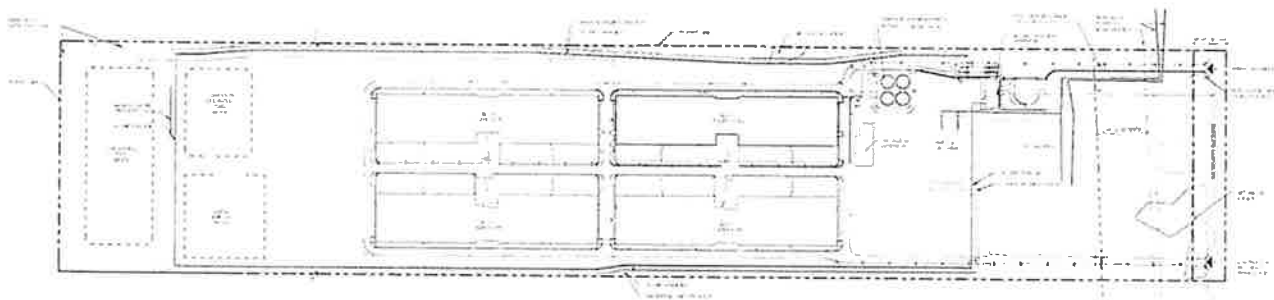
Major roadways, including highways, arterials, and collectors serve as the primary network for moving people and goods. These transportation corridors also provide access to businesses and homes and have served as the focus for commercial and residential development. If access points are not properly designed, these roadways will be unable to accommodate the needs of development and retain their primary transportation function. This ordinance balances the right of reasonable access to private property with the right of the citizens of Morrow County and the State of Oregon to safe and efficient travel.

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- A. Minimum Lot Frontage Requirement. Every lot shall abut a street, other than an alley, for at least 50 feet, except on cul-de-sacs where the frontage may be reduced to 30 feet.
  - a. Lot Frontage: 5,272.92 feet**
  
- B. Access Permit Requirement. Where access to or construction on a county road is needed, an access permit or right-of-way permit from Morrow County Public Works department is required subject to the requirements in this Ordinance. Where access to a state highway is needed, an access permit from ODOT is required as part of the land use application. Where access is needed to a road managed by the Forest Service or other entity, an access permit or other authorization from the appropriate entity shall be required as part of the land use application.
  - a. Access to Bombing Range Road is required. Bombing Range Road is identified as a county road. Applicant has obtained County approval of two access driveways on the subject property. See enclosed. Additionally, as illustrated on the master site plan, the primary access driveway is sufficiently deep to permit extensive on-site queueing, if required, which will alleviate the potential of queueing on Bombing Range Road.**
  
- C. Emergency Vehicle Access. It is the responsibility of the landowner to provide appropriate access for emergency vehicles at the time of development. A dead-end private street exceeding one hundred-fifty (150) feet in length shall have an adequate turn around facility approved by the appropriate Fire Marshal or, if the Fire Marshal fails to review the private street, approval by the Building Official or his designee.
  - a. The proposed development has an internal loop road which is adequately sized to allow emergency vehicular ingress and egress from the development.**

NOTES



CONFIDENTIAL

BOBBI RANCE ROAD  
BORUMAN, OR 97815

PERMIT SET

ZONING ARCHITECTURAL SITE PLAN

**SITE PLAN LEGEND**

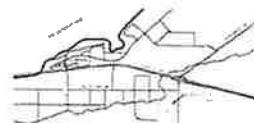
- Building Footprint
- Parking
- Landscape
- Utility
- Other

**GENERATOR COUNTS**

- Residential
- Commercial
- Industrial
- Office
- Public
- Other

**GENERAL DIMENSIONAL REGULATORY STANDARDS**

Item	Standard
Minimum Lot Area	10,000 sq. ft.
Minimum Lot Width	30 ft.
Minimum Front Setback	10 ft.
Minimum Side Setback	5 ft.
Minimum Rear Setback	5 ft.
Maximum Building Height	35 ft.
Maximum Floor Area Ratio (FAR)	0.5
Maximum Building Footprint Area	5,000 sq. ft.
Maximum Parking Ratio	1:100



CITYMAP

NOTES

1. ALL DIMENSIONS ARE IN FEET AND INCHES.

2. THE SHOWN DIMENSIONS ARE APPROXIMATE AND SHOULD BE USED AS A GUIDE ONLY.

3. THE SHOWN DIMENSIONS ARE SUBJECT TO CHANGE WITHOUT NOTICE.

4. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE LOCAL GOVERNMENT.

5. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE STATE OF OREGON.

6. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE FEDERAL GOVERNMENT.

7. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE INTERNATIONAL BUILDING CODE.

8. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE NATIONAL FIRE PROTECTION ASSOCIATION.

9. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF MECHANICAL ENGINEERS.

10. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF CIVIL ENGINEERS.

11. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF ELECTRICAL ENGINEERS.

12. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF MECHANICAL ENGINEERS.

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19. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF CIVIL ENGINEERS.

20. THE SHOWN DIMENSIONS ARE SUBJECT TO THE REQUIREMENTS OF THE AMERICAN SOCIETY OF ELECTRICAL ENGINEERS.

2001

## Tamra Mabbott

---

**From:** Jesse Walt <jessew@ygh.com>  
**Sent:** Monday, March 14, 2022 4:30 PM  
**To:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA); King, Seth J. (POR)  
**Cc:** Tamra Mabbott  
**Subject:** RE: AWS and NAS connect  
**Attachments:** Pages from PDX-AWS-00-ZZ-DR-E-0001 - Permit.pdf

Hello Kimberly,

I've attached a composite of the site lighting plan and fixture schedule from our pending permit submission (stamp redacted). I suspect you may need additional information, such as photometrics, but do not have that readily available.

Please let me know what additional information, if any, is required.

If any adjustments need to be made to the site lighting, can you please also supply a copy of the standards we need to meet? I've been unable to locate them online.

Thank you

Jesse Walt, AIA, LEED AP BD+C | Shareholder | he/him  
 Yost Grube Hall Architecture  
 707 SW Washington Street, Suite 1200 | Portland, OR 97205 | USA  
 t 503 221 0150 | d 503 715 3214 | m 503 449 2896 | w [ygh.com](http://ygh.com)



This e-mail is intended solely for the addressee. Access or use by anyone else is unauthorized and may be unlawful. Electronic information may be altered and cannot be guaranteed.

**From:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA) <kimberly.n.peacher.civ@us.navy.mil>  
**Sent:** Monday, March 14, 2022 3:48 PM  
**To:** King, Seth J. (POR) <sking@perkinscoie.com>; Jesse Walt <jessew@ygh.com>  
**Cc:** Tamra Mabbott <tmabbott@co.morrow.or.us>  
**Subject:** RE: AWS and NAS connect

Hello Seth,

Thank you for reaching out.

Jesse,

When ready, can you please forward the lighting study and or plans?

Thank you.

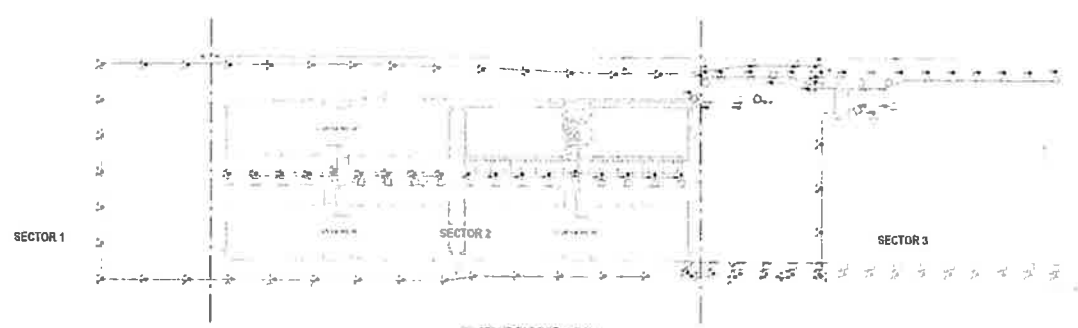
V/R,

Kimberly Peacher  
 Community Planning & Liaison Officer

**GENERAL NOTES**

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE IBC AND ALL APPLICABLE CODES AND REGULATIONS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL, STATE AND FEDERAL AGENCIES.
3. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION AND APPROVAL BY THE LOCAL BUILDING DEPARTMENT.
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL ADJACENT PROPERTIES AND UTILITIES AT ALL TIMES.
5. ALL UTILITIES SHALL BE PROTECTED AND MARKED PRIOR TO CONSTRUCTION.
6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ALL EXISTING UTILITIES AND STRUCTURES.
7. ALL WORK SHALL BE COMPLETED WITHIN THE SPECIFIED TIME FRAME.
8. THE CONTRACTOR SHALL MAINTAIN A NEAT AND ORDERLY WORK SITE AT ALL TIMES.
9. ALL MATERIALS SHALL BE STORED PROPERLY AND PROTECTED FROM THE ELEMENTS.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DISPOSAL OF ALL DEBRIS AND WASTE MATERIALS.

**SHEET NOTES**



500 LAYOUT GENERAL PLAN

NO.	DESCRIPTION	QUANTITY	UNIT	PRICE	TOTAL	REMARKS
10	EXTERIOR ARCHITECTURAL AREA LUMINAIRE - WELL MOUNT	277	LF	17	4709	INCLUDE WITH FUTURE VOLUMES PHOTOCELL
11	EXTERIOR ARCHITECTURAL AREA LUMINAIRE - WALL MOUNT	277	LF	27	7479	INCLUDE WITH FUTURE VOLUMES PHOTOCELL
12	EXTERIOR ARCHITECTURAL AREA LUMINAIRE - POLE MOUNT	277	LF	14	3878	INCLUDE WITH FUTURE VOLUMES PHOTOCELL

**CONFIDENTIAL**

BOMBING RANGE ROAD BOARDMAN, OR 97110

**PERMIT SET**



**REVISIONS**

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMITTING	10/15/2024
2	REVISIONS	10/15/2024
3	REVISIONS	10/15/2024
4	REVISIONS	10/15/2024
5	REVISIONS	10/15/2024
6	REVISIONS	10/15/2024
7	REVISIONS	10/15/2024
8	REVISIONS	10/15/2024
9	REVISIONS	10/15/2024
10	REVISIONS	10/15/2024

**DATE:** 10/15/2024  
**PROJECT:** BOMBING RANGE ROAD BOARDMAN, OR 97110  
**CLIENT:** [REDACTED]  
**DESIGNER:** [REDACTED]  
**CHECKER:** [REDACTED]  
**APPROVER:** [REDACTED]

02/22/2022

Tamra Mabbott  
Planning Director  
Morrow County

RE: Bombing Range Road Zoning Application

Dear Tamra,

The undersigned is the applicant requesting approval of a zoning permit application affecting Map No. 4N 25 24 Tax Lot 1701 (County File No. ZP-2956-22). The applicant understands that this matter is scheduled for consideration by the Planning Commission at a meeting later today. The applicant requests that the Planning Commission open the public hearing for this matter at its February 22, 2022 meeting and then immediately continue it to the Planning Commission's next meeting on March 29, 2022, to allow the applicant the opportunity to clarify aspects of the application materials. The applicant will appear at the continued hearing on March 29 to address the Planning Commission. Thank you for your consideration of this request.

Sincerely,



Jesse Walt  
Shareholder  
Yost Grube Hall Architecture

707 SW Washington St  
Suite 1200  
Portland, Oregon  
97205 USA  
t 503 221 0150  
f 503 295 0840  
w ygh.com

THOMAS F O'BRIEN, AIA  
ERICKA COLVIN, AIA  
PAUL KRUEGER, AIA  
STEVE NEIGER, AIA  
CRYSTAL SANDERSON, AIA  
KATHY SIMONE, AIA  
SCOTT THAYER, AIA  
JESSE WALT, AIA



February 21, 2022

Please place this comments into the record for ZP 2956-22

To the Morrow County Planning Commission,

It's unusual that we see this particular piece of land again in such a short period of time. If you recall this property was first introduced in a land swap application from the Port of Morrow. Where it was deemed to be isolated from industrial infrastructure including roadways, rail, water and sewer, utilities and industrial process lines and deemed un-feasible for any industrial purpose. In fact the application was justified, processed and approved by this Planning Commission to return this industrial land back into EFU.

Further the "Port" retained the right to convert the zoning from industrial to agricultural (Exhibit 1) this document was adopted into the Morrow County Comprehensive plan dated Oct 12, 2010. This land was never intended to be anything but a place holder of inventoried industrial land that could be exchanged at a later date. The land was intended to remain EFU. A zone that complements the surrounding area.

The zoning ownership has not been conveyed to the current property owner of record. Any permit at this time by this applicant is out of compliance with Morrow County Comprehensive Plan. A plan amendment must be completed before any permit is in order. The Port could remove the zoning at anytime leaving the application frustrated. This permit should be denied until that lift has been accomplished.

An urban use is defined as any residential, commercial, industrial, transit, transportation passenger facility, or retail use, or any combination of those uses. Urban uses are intended to be placed in urban areas for good reason. Is there any reasonable expectation for urbanization here? What is the future Public benefit? Amazon is asking for public facilities to be placed in a rural agricultural setting, and not only is this setting rural it is located adjacent to a subdivision.

The proposed 5 acre waste water plant is not something that should be located adjacent to a housing subdivision, building a fence around it does not minimize the stench, nor the stench it will produce when applied to the surrounding agricultural areas. It is these undisclosed amounts of applied water that are contaminating wells and poisoning our unborn and children with nitrates and heavy metals. That particular area is a hot spot according to the testing Lab and realtors alike.

Our Planning commission stumbles routinely on the difference between waste water and sewage. Nowhere in this application is there any reference to sewage disposal.

Morrow county does not support sewage treatment facilities according to Plan. Pg 183 MC ( Exhibit 2)

MC 3.070 A ) Uses permitted outright and accessory uses.

I do not believe that we can stretch the definition of a accessory use to include a security building. Nothing precludes this building to be located off shore.

### 3.070. C (1)

If our planning commission sees it fitting to place a large scale urban industrial plant next to a rural residential area and in the heart of agriculture. Clearly planning has gone out the window. The approval of this permit will violate our " County Plan", To plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development. This is supported by OAR 660-015-0000(11). It is also supported in the Industrial Element of our plan. " Industrial uses should not encroach on our residential or agricultural uses" Page 37 MC Comprehensive plan.( Exhibit 3)

The reasons, or justifications do not support the fact that we as a county have limited the size of a facility to 2 acres. The applicant has asked us approve this large scale urban facility on 120 plus acres, slightly over two.

You can't put enough lipstick on a pig to call it anything but a pig. The applicant has failed to give any substantial reasons to permit this size and scale next to a residentially zoned neighborhood and subdivision.

1.) Applicant states it meets the development criteria for M-G standards, however it does not meet the requirements of MC 3.070 C. Those M-G requirements can be met inside the urban growth boundary or within in the Port areas where this type of large scale facility should be sited.

2.) Applicant suggests the buildings or "primary data center use" will be near the west end of the project, however nothing precludes them from building on the entire premises or dividing off another parcel. Nothing provides evidence that the proposed accessory dwellings, security building or sewer ponds won't be located next to the residential area. As we know in land use a **map** is a mere suggestion. Nothing in the text supports this concept. And the map that was provided is so small its un readable.

A perimeter slatted fence and some shrubbery will not minimize acoustical or roof top mechanical equipment and lighting. This was the same song and dance that PDX 90 supported, bet you can't see or hear that eye- sore and its associated uses.

Data Centers operate 24/ 7 families and children sleep. The Zellers family that lives closest to PDX 90 can tell you about the volumes of traffic that flows day and night from workers and security. They are forced to live with the constant hum and the constant light that pierces their windows. These large scale facilities should be located in designed areas and not on islands that abut residential sites.

3.) The applicant points to the fact that access is from Bombing Range, but what precludes them from another access point off Miller. I believe PDX 90 was required to have both an entrance and exit for emergency service providers, not a turn around as proposed.

4.) Applicant states they "may" design it to minimize adverse impacts. That does not say shall or will. What does minimize mean anyway. In my humble opinion no dust, glare, traffic, odor, vapor or blasting is what our Morrow County plan provides to our residential areas. Not some minimized amount.

5.) Again lipstick, lipstick, lipstick!!!! You cannot hide a 126.92 acre data center campus.

The conditions of approval do not address odor, dust, water quality or quantity, sewer service or sewer facilities. Provides for minimal fencing and landscape and suggest should be designed and not will be. How will a goal 11 exception be met for water and sewer?

Timing and planning go hand in hand. Can we warrant the need for this development? Do we need more jobs? Can it be located elsewhere? Is it site specific? Does it meet our goals in our "Plan" Consideration needs to be applied our resources and public facilities. Water for this project is from municipalities, What are our reserves? How much Agricultural land will be condemned by UEC.

Here is what our plan says. " promote public health, safety and general welfare based on these considerations. MC plan pg 2 ( Exhibit 4)

1. The various characteristics of the various areas in the county, the suitability of the area for a particular land uses and improvements, the land uses and improvements in the area, property values, the needs of economic enterprises in the future development of the area, needed access to particular sites in the area, natural resources of the county and prospective needs for development thereof, and the public need for healthful, safe, aesthetic surroundings and conditions. (Page 2 MC Comp Plan)

Ask yourself how your Comprehensive Plan applies to this development.

1. Simply does not fit the character of the surrounding area.
2. There is better suitability in Oregon's 2nd largest port industrial area, shovel ready with all wet and dry utilities already provided.
3. No room for improvements as all other surrounding land is EFU.
4. Will drive property values of Ag lands up and residential properties down.
5. There is no future development of MG planned in the future of the area.
6. Several Data centers exist currently and none have contributed to our poverty level.
7. This project is in or near the critical ground water area and a known area of high nitrates.
8. And there isn't enough lipstick in the world to make this Data Center aesthetically pleasing.

### 3070.E. Traffic Impacts

A TIA is required for trips generated over 400. To simply compare an existing Data Center that may not have security buildings, storage tanks, water treatment facilities, data storage, and waste water holding ponds is not sufficient evidence that a TIA is not required. A traffic Impact study should be completed prior to the development , to factually asses the level of service, intersection project areas , peak levels and to address any mitigation that will be needed.

Clearly this permit application is premature and needs to be denied, given the fact that the applicant does not hold the ownership rights to the MG zoning that has been applied to this parcel. As stated above, an amendment to the comprehensive plan will be required before further action can be taken.

That is not the only reason this permit should be denied. Assess the damages to your neighbor, the condemnations of Ag land required for 230 KV lines, the resources consumed for what social value, property values, quality of life, and uphold the " Plan " that was crafted and put in place to eliminate what very permit would destroy. Flavor of the month planning results in unfavorable issues that remain for decades and become the struggles of future generations.

I'm sorry I cannot attend tonight, due to the fallout from the previous Amazon permitting process, I have to defend my property rights in a hearing being held at the same time in the City of Boardman.

Jonathan Tallman



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March 17, 2022

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**VIA EMAIL TO TMABBOTT@CO.MORROW.OR.US**

Jeff Wenholz, Chair  
Morrow County Planning Commission  
c/o Morrow County Planning Department  
Morrow County Government Center  
215 NE Main Ave  
Irrigon, OR 97844

**Re: Data Center Campus Zoning Permit Application  
Morrow County Planning Department File No. ZP-2956-22  
Applicant's Supplemental Application Materials and Request to Modify  
Conditions of Approval**

Dear Chair Wenholz and Members of the Planning Commission:

This office represents Amazon Data Services, Inc. ("ADS"), the applicant requesting Morrow County ("County") approval of a zoning permit for a data center campus, including related primary and accessory uses and buildings ("Project") styled as County File No. ZP-2956-22 ("Application") on approximately 126 acres of MG-zoned property identified as Tax Lot 1701 of Assessor's Map No. 4N 25E Section 24 ("Property").

**1. Supplemental Application Materials.**

Enclosed please find the following materials in support of the Application:

- Exhibit 1 - Letter from Port of Morrow dated March 16, 2022: This letter explains that the proposed Industrial Wastewater Pond on the Property will be for the sole purpose of collecting surge flow from the Project. It will not store organically loaded waters from food processing facilities and therefore will not have any odors.
- Exhibit 2 - Transportation Impact Analysis prepared by Parametrix: This exhibit includes a Transportation Impact Analysis ("TIA") for the Project prepared by

Morrow County Planning Commission

March 17, 2022

Page 2

licensed traffic engineers at Parametrix. In the TIA, Parametrix evaluated the projected trip impacts of the Project on nearby intersections and determined that, after build-out, trips associated with the Project would not cause any intersections to fail to meet County level of service standards. *Id.* Parametrix also determined that the site driveways would operate consistent with County level of service standards during the PM peak hour. *Id.* For these reasons, Parametrix determined that no off-site mitigation measures are recommended to the transportation system to offset the impacts of the Project. *Id.* The TIA further demonstrates how the trip distribution from the Project will generally be to the north on Bombing Range Road and will not be adversely impacting residentially-zoned properties, which are located to the northwest and on a different street network. *Id.*

- Exhibit 3 - Site Lighting Plan/Fixture Schedule and Coordination with U.S. Navy: This exhibit includes a composite of the Project lighting plan and fixture schedule, which illustrates that the outdoor lighting fixtures are directional and intended to only illuminate in a downward direction from their mounting location. This exhibit also includes evidence of ADS sharing this plan with the U.S. Navy to evaluate for potential glare to flight paths to/from the Bombing Range. The Navy representative responded that based upon the additional information included in this exhibit, “there are no concerns.”

## 2. Requested Modifications to Conditions of Approval.

On the basis of the enclosed materials, ADS requests the following modifications to the proposed conditions of approval in the staff report:

- Delete Condition 2 (requiring compliance with Navy light mitigation recommendations) on the grounds that Exhibit 3 demonstrates that the Navy has not recommended any lighting mitigation measures;
- Modify Condition 4 (requiring signage and signals at Project access points) to only require signage consistent with the recommendations set forth in the TIA by Parametrix; and

Morrow County Planning Commission

March 17, 2022

Page 3

- Delete Condition 5 (requiring construction of off-site transportation mitigation measures) because the TIA demonstrates that the projected traffic impacts of the Project will not warrant any such mitigation measures.

### 3. Conclusion.

Based upon these materials and the additional evidence and argument presented by ADS on the record, the Planning Commission should approve the Application, subject to staff's proposed conditions of approval, as modified by ADS' requests in this letter.

I have asked County Planning staff to place a copy of this submittal into the official record for this file and to place a copy before you. Thank you for your careful review of this information.



Seth J. King

Encls.

cc: Tamra Mabbott (via email) (w/encls.)  
Justin Nelson (via email) (w/encls.)  
Yost Grube Hall (via email) (w/encls.)  
Client (via email) (w/encls.)



March 16, 2022

Tamra Mabbott  
Planning Director  
205 Third Street NE  
Irrigon, OR 97844

Tax Lot 1701 on 4N 25E section 24 New Facility IWW Storage Pond

Dear Tamra,

This letter is in reference to some concerns with an IWW storage pond proposed as part of the new development on tax lot 1701 on map 4N 25E Section 24. The IWW storage pond located on the western side of this site will be operated by the Port of Morrow. The sole purpose of this pond is to accommodate surge flow from this facility. The POM will not use the lagoon to store any organically loaded waters from food processing facilities. IWW water from this facility is primarily cooling water and will not have any odors. I believe this will alleviate any concerns with regards to odor concerns to neighboring developments.

Thank You,

A handwritten signature in black ink, appearing to read "Jacob K Cain", is written over the typed name and title.

Jacob K Cain, P.E.  
Director of Engineering



# Bombing Range Road Data Center TIA

Prepared for  
Amazon Web Services (AWS)

March 2022

Prepared by  
**Parametrix**

Exhibit 2  
Page 1 of 63



# Bombing Range Road Data Center TIA

*Prepared for*

**Amazon Web Services (AWS)**

*Prepared by*

**Parametrix**

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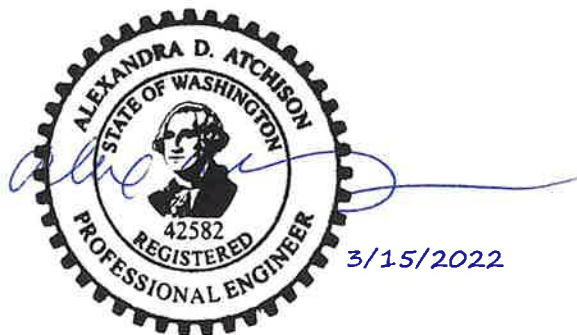
[www.parametrix.com](http://www.parametrix.com)

## CITATION

Parametrix, 2022. Bombing Range Road Data Center TIA.  
Prepared by Parametrix, Seattle, Washington. March 2022.

# CERTIFICATION

The technical material and data contained in this document were prepared under the supervision and direction of the undersigned, whose seal, as a professional engineer licensed to practice as such, is affixed below.



Agustin Castro  
Prepared by Agustin Castro, EIT

Alex Atchison  
Checked by Alex Atchison, PE

Dwight Miller  
Approved by Dwight Miller, PE

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- A Volumes
- B Synchro HCM 6 Reports



## ACRONYMS AND ABBREVIATIONS

30HV	30th highest hourly volume
APM	<i>Analysis Procedures Manual</i>
ATR	automatic traffic recorder
IAMP	interchange area management plan
ITE	Institute of Transportation Engineers
LOS	level of service
mph	miles per hour
ODOT	Oregon Department of Transportation
sf	square feet
TIA	transportation impact analysis
TMC	turning movement counts
TSP	transportation system plan
TWSC	two-way-stop-controlled
UGB	Urban Growth Boundary
v/c	volume-to-capacity



## 1. INTRODUCTION

The purpose of this transportation impact analysis (TIA) is to identify potential transportation-related impacts to the surrounding streets network associated with the proposed data center located off Bombing Range Road in Morrow County, Oregon.

## 2. PROJECT DESCRIPTION AND STUDY AREA

The proposed project includes four data center buildings, each totaling 215,461 square feet (sf) and one security building, approximately 6,560 sf in size, for a total of 868,404 sf. A total of 218 parking stalls will be provided on-site. The site will also include a substation and industrial wastewater pond. The proposed project is anticipated to be constructed by 2024.

Figure 1 shows the project location and the intersections analyzed as part of this TIA. The scope of the analysis was coordinated with Morrow County staff. Based on the anticipated travel patterns for the project-generated vehicle traffic, the following off-site intersections were selected for study and are shown in Figure 1.

1. US 730/Interstate 84 (I-84) westbound ramps
2. US 730/I-84 eastbound off-ramp
3. Bombing Range Road/I-84 eastbound on-ramp
4. Frontage Road and Bombing Range Road
5. Wilson Lane and Bombing Range Road

The study focuses on the weekday PM peak hour. The future 2024 horizon year is evaluated consistent with when the proposed project is anticipated to be constructed and occupied. The analysis includes a review of the roadway network, non-motorized facilities, transit service, existing and future peak hour traffic volumes, traffic operations, and traffic safety. Future with-project conditions are evaluated by adding site-generated traffic to future without-project volumes. Future without-project and with-project conditions were compared to identify the relative impacts the proposed project would have on the surrounding transportation system.



Figure 1. Study Area and Study Intersections

### 3. EXISTING AND FUTURE WITHOUT-PROJECT CONDITIONS

This section describes both existing and future (2024) without-project conditions within the identified study area. Study area characteristics are provided for the roadway network, planned improvements, transit service, non-motorized facilities, existing and future forecasted without-project traffic volumes, traffic operations, and safety.

#### 3.1 Street Network

Main and secondary access to the site is proposed via Bombing Range Road. Bombing Range Road is a two-lane major collector that provides access to I-84 and US 730<sup>1</sup>. US 730 is classified as a regional

<sup>1</sup> Morrow County classifies Bombing Range Road as a Major Collector (Figure 3-1, Morrow County Transportation System Plan, 2012).

highway and connects Boardman and the Port of Morrow to the nearby towns of Irrigon and Umatilla.<sup>2</sup> Within the study area, US 730 is a two-lane highway with a posted speed limit of 55 miles per hour (mph). The roadways described above provide access to I-84, the major interstate in Boardman and in this area of Morrow County. All intersections within and adjacent to the project area are unsignalized and two-way-stop-controlled (TWSC).

Morrow County's most recent transportation system plan (TSP) includes projects with varying priorities dated 2017 to 2022. None of the noted projects (high- or medium-priority) are located on county roads in the project study area.

Oregon Department of Transportation's (ODOT's) Transportation Project Tracker<sup>3</sup> tracking map notes two active projects in the study area. The first project is in the design phase and would replace signage along I-84 and I-82. The second project is in the construction phase and would replace and/or repair culverts along I-84 and US 730. None of the noted ODOT projects would affect roadway capacity or intersection control in the study area; therefore, neither project was modeled in future conditions.

## 3.2 Non-Motorized Facilities

Due to the rural nature of the study area, exclusive pedestrian and bicycle facilities (e.g., sidewalks and bike lanes) are not present in the study area; however, some of the study roadways have shoulders that are at least partially paved and provide additional space for autos and bicycles to share the roadway. Traffic volumes are also relatively low on the study area roadways, making it more comfortable for bicycles and autos to share the roadway. There are no dedicated equestrian facilities within the project area.

## 3.3 Transit

There is no transit service in the study area.

## 3.4 Traffic Volumes

The following sections document the development of the traffic volumes used in the existing and future without-project operations analyses.

### 3.4.1 Existing Traffic Volumes

Existing traffic volumes are based, in part, on turning movement counts (TMC) collected near the proposed project on Tuesday October 8, 2019, for 16 consecutive hours, consistent with Oregon's *Analysis Procedures Manual (APM)*.

TMCs were collected at four intersections:

1. US 730/Lewis and Clark Road
2. Columbia Drive/Lewis and Clark Road

---

<sup>2</sup> ODOT classifies US 730 as a regional highway while Morrow County classifies the roadway as a Minor Arterial (Figure 3-1, Morrow County Transportation System Plan, 2012).

<sup>3</sup> ODOT. Transportation Project Tracker. <https://gis.odot.state.or.us/tpt/>

3. Laurel Lane/I-84 eastbound Ramp
4. Laurel Lane/I-84 eastbound Ramp

Counts were not collected at intersections in the study area; therefore, existing volumes at these locations were estimated based on counts taken in 2010 as part of the I-84/US 730 Interchange Area Management Plan (Kittelson & Associates Inc.) and the Port of Morrow Interchange Area Management Plan (Kittelson & Associates Inc.). To estimate 2021 counts at the study area intersections, the following methodology was used:

- Compared traffic volumes on US 730, north of I-84, collected in 2019 to traffic volumes on US 730 collected in 2010; estimated annual growth rate.
- Compared traffic volumes at the Laurel Lane/I-84 EB Ramp and Laurel Lane/I-84 EB Ramp, collected in 2019, to traffic volumes at these ramps collected in 2010; estimated annual growth rate.
- The average annual growth rate was found to be 3.5%. This growth rate was applied to the 2010 existing volumes at the study area intersections listed above. The 2010 existing volumes used were included in the I-84/US 730 Interchange Area Management Plan.

The 3.5% annual growth rate used to estimate existing volumes at the study area intersections was compared to ODOT's recommended historical growth rate using automatic traffic recorder (ATR) sites in the surrounding project area. The 2017-2019 data from ATR #25-008 located on I-84, east of the interchange with US 730, equates to a 2.2% growth rate, which is less conservative than the proposed 3.5% annual growth rate.

ODOT's *APM* requires a seasonal adjustment factor that adjusts count month to the peak month as well as using the seasonal trend table. Consistent with ODOT's *APM*, the original data presented in the interchange area management plans (IAMPs) were modified to 30th highest hourly volume (30HV) conditions by applying a seasonal adjustment factor. Therefore, an additional seasonal factor was not applied to estimate existing volumes, just the annual growth rate. Additionally, no historical growth factor was applied between the developed 2019 volumes and the 2021 existing volumes as it was assumed no growth occurred during COVID-affected travel patterns. The existing weekday PM peak period traffic counts and volume information from the IAMPs are included in Appendix A.

### 3.4.2 Future Without-Project Traffic Volumes

Future (2024) without-project traffic volumes were forecasted using the same annual growth rate of 3.5%, as described above.

## 3.5 Safety

Between 2016 and 2021, three reported collisions occurred at the study area intersections. All collisions occurred around the US 730/I-84 interchange intersections. No intersections were identified as high accident locations. Data was collected using ODOT Crash Reports on mainline and connection sections for I-84 and US 730, as well as Bombing Range Road and Wilson Lane.

The three crashes were composed of two fixed-object types and one angled collision. Two incidents, both located on the US 730 interchange-related intersection, had at least one Class C Injury. The US 730 interchange related collisions were fixed object, and angled collision. One other injury occurred on the I-84 eastbound on-ramp and had no injury designation. The type for the I-84 collision was fixed object.

## 3.6 Traffic Operations

The following summarizes traffic operations for existing and future without-project conditions for the study area intersections.

All operational analyses described in this report were performed in accordance with the procedures and methodology of the Transportation Research Board's *Highway Capacity Manual* (6th Edition). Operational standards are based on volume-to-capacity (v/c) ratios. The operational standards within the study area differ depending on the jurisdiction. Three intersections (Lewis and Clark Drive/US 730, and both I-84 interchanges) are on ODOT facilities and have v/c ratio standards based on Table 3-9 of the Morrow County TSP.<sup>4</sup> Because the Lewis and Clark/US 730 intersection is outside the Boardman Urban Growth Boundary (UGB), it has a peak hour v/c ratio standard of 0.70. The I-84 interchanges also have v/c ratio standards of 0.70 regardless of UGB presence.

The operational standards for intersections only involving County roads are based on level of service (LOS). The LOS is a scale ranging from A to F in which rankings are based on the delay at a given intersection. LOS A represents the best conditions with minimal amount of delay while LOS F represents the worst conditions with severe congestion and delay. Table 1 lists the intersection LOS delay thresholds for signalized intersections and unsignalized intersections. At signalized and all-way stop-control intersections, LOS is calculated based on the delay of all vehicles entering the intersection. At two-way or one-way stop-control intersections, LOS is calculated and reported based on the worst movement at the intersection.

**Table 1. Highway Capacity Manual LOS Ratings**

Level of Service	Average Delay (seconds/vehicle) Signalized Intersections	Average Delay (seconds/vehicle) Unsignalized Intersections
A	≤ 10	≤ 10
B	> 10 and ≤ 20	> 10 and ≤ 15
C	> 20 and ≤ 35	> 15 and ≤ 25
D	> 35 and ≤ 55	> 25 and ≤ 35
E	> 55 and ≤ 80	> 35 and ≤ 50
F	> 80	> 50

Source: *Highway Capacity Manual* (6th Edition), Transportation Research Board, 2016.

The County's standard is LOS "C" or better for unincorporated areas (i.e., intersections along Bombing Range Road).

Weekday PM peak traffic operations were analyzed for existing and future without-project conditions using Synchro v11. Synchro 11 is a macroscopic analysis and optimization software application that supports the methodology of the Transportation Research Board's *Highway Capacity Manual* (6th Edition). Results for the existing and future without-project operations analyses are summarized in

<sup>4</sup> Table 3-9 of the Morrow County TSP references the Oregon Highway Plan (1999; last updated 2015) as the source of the standards.

Table 2. Detailed LOS worksheets for each study intersection are included in Appendix B. All intersections operate within operational standards during both the existing and future without-project conditions.

**Table 2. PM Peak Hour LOS for Existing and Future Without-Project Conditions**

(#) Intersection	Existing Conditions			No Build Conditions		
	LOS <sup>a</sup>	Delay (sec/veh) <sup>b</sup>	v/c Ratio	LOS	Delay (sec/veh)	v/c Ratio
US 730/I-84 Westbound Ramps	B	10.5	0.05	B	10.8	0.05
US 730/I-84 Eastbound Off-Ramp	A	9.7	0.08	A	9.9	0.09
I-84 Eastbound On-Ramp and Bombing Range Road	–	–	–	–	–	–
Frontage Lane and Bombing Range Road	A	9.1	0.01	A	9.2	0.01
Wilson Lane and Bombing Range Road	A	9.3	0.06	A	9.4	0.07

a Level of Service (LOS), delay, and volume to capacity (v/c) ratio are reported for the worst movement at all intersections.

b Delay (sec/veh) = average seconds of delay per vehicle.

## 4. PROJECT CONDITIONS

This section documents the proposed project's impacts on the surrounding transportation network. First, estimated traffic generated by the proposed site are distributed and assigned to adjacent streets and intersections within the study area for the weekday PM peak hour study period. Next, 2024 future with-project traffic volumes are projected and potential impact to traffic operations are identified.

### 4.1 Trip Generation

Trip generation estimates were prepared for the proposed data center project based on trip rates identified using the Institute of Transportation Engineers (ITE) *Trip Generation Manual, 11th edition* (2021). Trip rates consistent with ITE Data Center (LU#160) were used for these estimates. The ITE rates use total square footage of the building as the variable to estimate trip generation. Using square footage as the variable, the proposed project is expected to generate approximately 860 daily trips. Another way to estimate daily trips would be to look at the number of trips per employee. The data center is expected to have a total of 302 employees. Given the rural nature of the project site, it is unlikely that each employee would make more than two trips to the site, inbound at the beginning of their shift and outbound at the end of their shift, equally 604 daily trips. The ITE trip rates are likely overestimated trips, especially daily trips. However, to be conservative, the ITE trip rate was used to estimate expected trips generated during the PM peak hour.

As shown in Table 3, the project is estimated to generate approximately 78 new PM peak hour trips with 23 inbound and 55 outbound. Table 3 details the proposed trip generation.



**Table 3. Estimated PM Peak Hour Trip Generation**

Land Use	Units	Trip Rate	PM Peak Hour Trips		
			In	Out	Total
Data Center (LU #160)	868,404 sf	0.09 trips/1,000 sf	23	55	78

## 4.2 Traffic Distribution and Assignment

Trip distribution patterns for the project were based on existing vehicle travel patterns as well as discussion with Morrow County staff. Figure 2 shows the trip assignment of project trips in the study area.



**Figure 2. Proposed Trip Distribution and Assignment**

## 4.3 Traffic Operations Impact

A future with-project LOS analysis was conducted for the weekday PM peak hour to analyze traffic impacts of the proposed project. The same methodologies were applied as used for existing and future without-project conditions. All intersection parameters such as channelization and intersection control were consistent with those used in the evaluation of future without-project conditions. A comparison of horizon year 2024 future without- and with-project weekday PM peak hour traffic operations are summarized in Table 4. Detailed LOS worksheets are provided in Appendix B

**Table 4. PM Peak Hour LOS for Existing and Future Conditions**

(#) Intersection	Existing Conditions			No Build Conditions			Build Conditions			
	LOS <sup>a</sup>	Delay (sec/veh) <sup>b</sup>	v/c Ratio	LOS	Delay (sec/veh)	v/c Ratio	LOS	Delay (sec/veh)	v/c Ratio	LOS or v/c Standard
US 730/I-84 Westbound Ramps	B	10.5	0.05	B	10.8	0.05	B	11.2	0.06	0.70
US 730/I-84 Eastbound Off-Ramp	A	9.7	0.08	A	9.9	0.09	B	10.3	0.16	0.70
I-84 Eastbound On-Ramp and Bombing Range Road	-	-	-	-	-	-	-	-	-	-
Frontage Lane and Bombing Range Road	A	9.1	0.01	A	9.2	0.01	A	9.6	0.01	0.70
Wilson Lane and Bombing Range Road	A	9.3	0.06	A	9.4	0.07	A	9.8	0.07	0.70

a Level of Service (LOS), delay, and volume to capacity (v/c) ratio are reported for the worst movement at all intersections.

b Delay (sec/veh) = average seconds of delay per vehicle.

With the addition of project-generated traffic during the weekday PM peak hour, all study intersections are forecast to meet their applicable operational standard in the 2024 with-project conditions.

## 4.4 Site Access

Main and secondary access to the site is proposed via Bombing Range Road. However, operational analysis of the site driveways was conducted assuming the most conservative case with 78 PM peak hour trips assigned to the main site driveway. A stop sign was assumed at the driveway and no intersection control was assumed on Bombing Range Road.

Results of the 2024 with-project analysis show that the site access driveway with Bombing Range Road will operate at LOS A, with 9.5 seconds of delay. Queuing is expected to be one car or less at the driveway. No turn lanes at this intersection are needed.

The driveway will be designed to standard, incorporating appropriate sight distance and turning radii.

## 4.5 Transportation Impacts

Because all project intersections are expected to operate within LOS and v/c standards mandated by ODOT and Morrow County in the 2024 with-project condition, mitigation efforts are not required.

## 5. FINDINGS AND CONCLUSIONS

This analysis summarizes the project traffic impacts of the proposed data center off Bombing Range Road in Morrow County, Oregon. General findings and recommendations include:

- The project would construct four data center buildings, each totaling 215,461 sf and one security building, approximately 6,560 sf in size, for a total of 868,404 sf. A total of 218 parking stalls will be provided on-site.
- The proposed project is anticipated to be constructed by 2024.
- The development is expected to generate approximately 78 new PM peak hour trips.
- With the addition of the project trips, the off-site study intersections are forecast to meet their applicable operational standard in the 2024 with-project conditions.

## 6. REFERENCES

- ITE (Institute of Transportation Engineers). 2021. Trip Generation Manual, 11th edition. Institute of Transportation Engineers, Washington, DC.
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# Appendix A

## Volumes







Data Provided by K-D-N.com 503-594-4224	
N/S street:	US 730
EW street:	Lewis and Clark Dr
City, State	Boardman OR
Study ID #	
Location	45,847331 - -119,597494
Start Date	Tuesday, October 08, 2019
Start Time	06:00:00 AM
Peak Hour Start	03:50:00 PM
Peak 15 Min Start	04:30:00 PM
PHF (15-Min Int)	0.69

Peak-Hour Volumes (PHV)																							
Northbound				Southbound				Eastbound				Westbound				Entering				Leaving			
Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	NB	SB	EB	WB	NB	SB	EB	WB
16	187	0	0	0	93	18	0	155	0	96	0	0	0	0	0	203	111	251	0	189	342	34	0

Percent Heavy Vehicles																							
31.3%	8.0%	0.0%	0.0%	0.0%	9.7%	38.9%	0.0%	5.8%	0.0%	7.3%	0.0%	0.0%	0.0%	0.0%	0.0%	9.9%	14.4%	6.4%	0.0%	8.5%	7.0%	35.3%	0.0%

PHV - Bicycles														PHV - Pedestrians							
Northbound				Southbound				Eastbound				Westbound				Sum	in Crosswalk				
Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn		NB	SB	EB	WB	Sum
0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Time	All Vehicle Volumes																15 Min	1 HR
	Northbound				Southbound				Eastbound				Westbound					
	US 730				US 730				Lewis and Clark Dr				Lewis and Clark Dr					
	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Sum	Sum
06:00:00 AM	4	4	0	0	0	8	4	0	0	0	0	0	0	0	0	0		
06:05:00 AM	4	5	0	0	0	14	5	0	2	0	0	0	0	0	0	0		
06:10:00 AM	3	4	0	0	0	12	5	0	3	0	1	0	0	0	0	0	78	
06:15:00 AM	8	5	0	0	0	11	12	0	4	0	0	0	0	0	0	0	98	
06:20:00 AM	7	7	0	0	0	14	5	0	3	0	0	0	0	0	0	0	104	
06:25:00 AM	3	6	0	0	0	17	6	0	3	0	0	0	0	0	0	0	111	
06:30:00 AM	7	7	0	0	0	25	12	0	1	0	4	0	0	0	0	0	127	
06:35:00 AM	5	4	0	0	0	18	9	0	0	0	5	0	0	0	0	0	132	
06:40:00 AM	3	3	0	0	0	27	14	0	1	0	1	0	0	0	0	0	146	
06:45:00 AM	6	6	0	0	0	25	17	0	2	0	3	0	0	0	0	0	149	
06:50:00 AM	1	6	0	0	0	16	7	0	3	0	1	0	0	0	0	0	142	
06:55:00 AM	4	5	0	0	0	10	3	0	3	0	1	0	0	0	0	0	119	454
07:00:00 AM	8	4	0	0	0	11	7	0	0	0	0	0	0	0	0	0	90	464
07:05:00 AM	5	7	0	0	0	9	6	0	2	0	0	0	0	0	0	0	85	463
07:10:00 AM	6	13	0	0	0	14	2	0	1	0	1	0	0	0	0	0	96	472
07:15:00 AM	2	7	0	0	0	13	3	0	3	0	1	0	0	0	0	0	95	461
07:20:00 AM	4	5	0	0	0	11	4	0	2	0	1	0	0	0	0	0	93	452
07:25:00 AM	3	12	0	0	0	9	10	0	1	0	1	0	0	0	0	0	92	453
07:30:00 AM	5	10	0	0	0	17	3	0	2	0	0	0	0	0	0	0	100	434
07:35:00 AM	2	9	0	0	0	11	4	0	0	0	1	0	0	0	0	0	100	420
07:40:00 AM	2	4	0	0	0	21	7	0	1	0	1	0	0	0	0	0	100	407
07:45:00 AM	5	13	0	0	0	9	11	0	1	0	0	0	0	0	0	0	102	387
07:50:00 AM	5	11	0	0	0	8	11	0	1	0	0	0	0	0	0	0	111	389
07:55:00 AM	9	8	0	0	0	2	3	0	1	0	2	0	0	0	0	0	100	388
08:00:00 AM	5	5	0	0	0	3	7	0	0	0	2	0	0	0	0	0	83	380



KEY DATA NETWORK

08:05:00 AM	2	5	0	0	0	7	6	0	1	0	1	0	0	0	0	0	69	373
08:10:00 AM	2	8	0	0	0	15	3	0	0	0	3	0	0	0	0	0	75	367
08:15:00 AM	3	6	0	0	0	3	1	0	5	0	1	0	0	0	0	0	72	357
08:20:00 AM	2	9	0	0	0	9	2	0	2	0	0	0	0	0	0	0	74	354
08:25:00 AM	0	5	0	0	0	5	2	0	0	0	2	0	0	0	0	0	57	332
08:30:00 AM	1	3	0	0	0	7	3	0	0	0	1	0	0	0	0	0	53	310
08:35:00 AM	4	8	0	0	0	6	2	0	2	0	1	0	0	0	0	0	52	306
08:40:00 AM	2	1	0	0	0	5	3	0	1	0	1	0	0	0	0	0	51	283
08:45:00 AM	2	6	0	0	0	8	2	0	2	0	3	0	0	0	0	0	59	267
08:50:00 AM	1	4	0	0	0	10	1	0	2	0	1	0	0	0	0	0	55	250
08:55:00 AM	4	6	0	0	0	2	5	0	0	0	4	0	0	0	0	0	63	246
09:00:00 AM	0	4	0	0	0	5	1	0	1	0	1	0	0	0	0	0	52	236
09:05:00 AM	1	2	0	0	0	4	4	0	0	0	2	0	0	0	0	0	46	227
09:10:00 AM	1	8	0	0	0	4	0	0	2	0	3	0	0	0	0	0	43	214
09:15:00 AM	4	4	0	0	0	5	1	0	2	0	1	0	0	0	0	0	48	212
09:20:00 AM	0	3	0	0	0	5	4	0	3	0	1	0	0	0	0	0	51	204
09:25:00 AM	1	7	0	0	0	5	2	0	3	0	3	0	0	0	0	0	54	211
09:30:00 AM	0	4	0	0	0	4	4	0	3	0	3	0	0	0	0	0	55	214
09:35:00 AM	3	7	0	0	0	3	1	0	1	0	0	0	0	0	0	0	54	206
09:40:00 AM	3	8	0	0	0	4	0	0	1	0	1	0	0	0	0	0	50	210
09:45:00 AM	1	3	0	0	0	7	4	0	3	0	1	0	0	0	0	0	51	206
09:50:00 AM	0	6	0	0	0	3	5	0	1	0	1	0	0	0	0	0	52	203
09:55:00 AM	1	3	0	0	0	2	1	0	0	0	1	0	0	0	0	0	43	190
10:00:00 AM	1	8	0	0	0	6	2	0	0	0	3	0	0	0	0	0	44	198
10:05:00 AM	1	6	0	0	0	4	5	0	2	0	0	0	0	0	0	0	46	203
10:10:00 AM	0	5	0	0	0	2	2	0	3	0	3	0	0	0	0	0	53	200
10:15:00 AM	0	4	0	0	0	6	3	0	2	0	2	0	0	0	0	0	50	200
10:20:00 AM	1	3	0	0	0	8	3	0	3	0	3	0	0	0	0	0	53	205
10:25:00 AM	5	5	0	0	0	1	0	0	3	0	1	0	0	0	0	0	53	199
10:30:00 AM	2	3	0	0	0	8	2	0	0	0	2	0	0	0	0	0	53	198
10:35:00 AM	4	3	0	0	0	5	0	0	7	0	2	0	0	0	0	0	53	204
10:40:00 AM	1	2	0	0	0	4	0	0	2	0	2	0	0	0	0	0	49	198
10:45:00 AM	1	4	0	0	0	7	2	0	6	0	2	0	0	0	0	0	54	201
10:50:00 AM	0	2	0	0	0	10	2	0	1	0	1	0	0	0	0	0	49	201
10:55:00 AM	2	12	0	0	0	1	2	0	2	0	4	0	0	0	0	0	61	216
11:00:00 AM	2	3	0	0	0	11	1	0	2	0	3	0	0	0	0	0	61	218
11:05:00 AM	2	7	0	0	0	6	3	0	1	0	2	0	0	0	0	0	66	221
11:10:00 AM	0	14	0	0	0	9	2	0	2	0	0	0	0	0	0	0	70	233
11:15:00 AM	3	3	0	0	0	3	1	0	1	0	2	0	0	0	0	0	61	229
11:20:00 AM	0	6	0	0	0	5	2	0	6	0	7	0	0	0	0	0	66	234
11:25:00 AM	1	3	0	0	0	7	4	0	0	0	0	0	0	0	0	0	54	234
11:30:00 AM	0	5	0	0	0	2	2	0	1	0	2	0	0	0	0	0	53	229
11:35:00 AM	0	7	0	0	0	6	3	0	2	0	2	0	0	0	0	0	47	228
11:40:00 AM	0	4	0	0	0	9	0	0	3	0	3	0	0	0	0	0	51	236
11:45:00 AM	0	9	0	0	0	2	0	0	0	0	2	0	0	0	0	0	52	227
11:50:00 AM	1	10	0	0	0	7	1	0	4	0	1	0	0	0	0	0	56	235
11:55:00 AM	6	8	0	0	0	7	1	0	5	0	1	0	0	0	0	0	65	240



# K-D-N

## KEY DATA NETWORK

12:00:00 PM	4	3	0	0	0	6	3	0	7	0	2	0	0	0	0	0	77	243
12:05:00 PM	3	8	0	0	0	5	4	0	6	0	2	0	0	0	0	0	81	250
12:10:00 PM	2	6	0	0	0	6	3	0	1	0	1	0	0	0	0	0	72	242
12:15:00 PM	0	7	0	0	0	9	2	0	3	0	1	0	0	0	0	0	69	251
12:20:00 PM	2	7	0	0	0	8	3	0	2	0	1	0	0	0	0	0	64	248
12:25:00 PM	4	8	0	0	0	2	1	0	1	0	0	0	0	0	0	0	61	249
12:30:00 PM	3	5	0	0	0	9	1	0	0	0	0	0	0	0	0	0	57	255
12:35:00 PM	2	9	0	0	0	3	1	0	1	0	3	0	0	0	0	0	53	254
12:40:00 PM	1	4	0	0	0	7	4	0	2	0	0	0	0	0	0	0	55	253
12:45:00 PM	1	5	0	0	0	4	0	0	0	0	0	0	0	0	0	0	47	250
12:50:00 PM	2	4	0	0	0	7	0	0	1	0	0	0	0	0	0	0	42	240
12:55:00 PM	0	9	0	0	0	4	2	0	0	0	1	0	0	0	0	0	40	228
01:00:00 PM	2	4	0	0	0	6	1	0	0	0	1	0	0	0	0	0	44	217
01:05:00 PM	1	7	0	0	0	2	4	0	3	0	0	0	0	0	0	0	47	206
01:10:00 PM	0	6	0	0	0	6	0	0	3	0	1	0	0	0	0	0	47	203
01:15:00 PM	1	11	0	0	0	2	3	0	5	0	0	0	0	0	0	0	55	203
01:20:00 PM	1	8	0	0	0	2	1	0	2	0	2	0	0	0	0	0	54	196
01:25:00 PM	1	7	0	0	0	3	4	0	2	0	2	0	0	0	0	0	57	199
01:30:00 PM	1	5	0	0	0	9	1	0	1	0	1	0	0	0	0	0	53	199
01:35:00 PM	0	8	0	0	0	5	4	0	6	0	5	0	0	0	0	0	65	208
01:40:00 PM	3	5	0	0	0	9	2	0	3	0	1	0	0	0	0	0	69	213
01:45:00 PM	3	7	0	0	0	6	6	0	3	0	2	0	0	0	0	0	78	230
01:50:00 PM	1	6	0	0	0	5	2	0	2	0	3	0	0	0	0	0	69	235
01:55:00 PM	2	11	0	0	0	10	0	0	0	0	2	0	0	0	0	0	71	244
02:00:00 PM	0	4	0	0	0	5	3	0	4	0	3	0	0	0	0	0	63	249
02:05:00 PM	4	9	0	0	0	6	6	0	1	0	4	0	0	0	0	0	74	262
02:10:00 PM	2	9	0	0	0	6	2	0	3	0	3	0	0	0	0	0	74	271
02:15:00 PM	0	4	0	0	0	7	4	0	1	0	2	0	0	0	0	0	73	267
02:20:00 PM	2	3	0	0	0	8	4	0	2	0	3	0	0	0	0	0	65	273
02:25:00 PM	1	5	0	0	0	9	5	0	7	0	6	0	0	0	0	0	73	287
02:30:00 PM	3	9	0	0	0	5	3	0	9	0	1	0	0	0	0	0	85	299
02:35:00 PM	2	9	0	0	0	7	6	0	12	0	7	0	0	0	0	0	106	314
02:40:00 PM	5	10	0	0	0	16	3	0	7	0	4	0	0	0	0	0	118	336
02:45:00 PM	1	13	0	0	0	15	3	0	3	0	5	0	0	0	0	0	128	349
02:50:00 PM	2	8	0	0	0	7	2	0	3	0	1	0	0	0	0	0	108	353
02:55:00 PM	0	4	0	0	0	3	0	0	7	0	6	0	0	0	0	0	83	348
03:00:00 PM	0	16	0	0	0	8	1	0	14	0	2	0	0	0	0	0	84	370
03:05:00 PM	2	15	0	0	0	7	0	0	11	0	4	0	0	0	0	0	100	379
03:10:00 PM	2	21	0	0	0	15	2	0	6	0	1	0	0	0	0	0	127	401
03:15:00 PM	0	22	0	0	0	8	2	0	4	0	7	0	0	0	0	0	129	426
03:20:00 PM	2	13	0	0	0	5	1	0	7	0	2	0	0	0	0	0	120	434
03:25:00 PM	3	16	0	0	0	12	0	0	6	0	4	0	0	0	0	0	114	442
03:30:00 PM	2	7	0	0	0	8	3	0	7	0	5	0	0	0	0	0	103	444
03:35:00 PM	3	17	0	0	0	9	2	0	8	0	4	0	0	0	0	0	116	444
03:40:00 PM	2	11	0	0	0	8	0	0	13	0	0	0	0	0	0	0	109	433
03:45:00 PM	3	18	0	0	0	9	1	0	5	0	4	0	0	0	0	0	117	433
03:50:00 PM	0	17	0	0	0	11	3	0	3	0	1	0	0	0	0	0	109	445

# K-D-N

## KEY DATA NETWORK

03:55:00 PM	2	10	0	0	0	11	1	0	6	0	3	0	0	0	0	0	108	458
04:00:00 PM	1	6	0	0	0	5	1	0	5	0	2	0	0	0	0	0	88	437
04:05:00 PM	5	16	0	0	0	4	2	0	14	0	5	0	0	0	0	0	99	444
04:10:00 PM	1	13	0	0	0	4	1	0	10	0	5	0	0	0	0	0	100	431
04:15:00 PM	3	18	0	0	0	9	1	0	14	0	5	0	0	0	0	0	130	438
04:20:00 PM	0	22	0	0	0	8	3	0	19	0	3	0	0	0	0	0	139	463
04:25:00 PM	3	11	0	0	0	9	1	0	12	0	8	0	0	0	0	0	149	466
04:30:00 PM	0	18	0	0	0	5	2	0	32	0	25	0	0	0	0	0	181	516
04:35:00 PM	0	19	0	0	0	9	2	0	21	0	20	0	0	0	0	0	197	544
04:40:00 PM	1	22	0	0	0	7	0	0	13	0	10	0	0	0	0	0	206	563
04:45:00 PM	0	15	0	0	0	11	1	0	6	0	9	0	0	0	0	0	166	565
04:50:00 PM	0	9	0	0	0	9	2	0	11	0	1	0	0	0	0	0	127	562
04:55:00 PM	0	4	0	0	0	9	0	0	0	0	4	0	0	0	0	0	91	546
05:00:00 PM	0	12	0	0	0	9	3	0	4	0	2	0	0	0	0	0	79	556
05:05:00 PM	0	12	0	0	0	6	0	0	3	0	4	0	0	0	0	0	72	535
05:10:00 PM	0	16	0	0	0	2	1	0	8	0	4	0	0	0	0	0	86	532
05:15:00 PM	0	14	0	0	0	6	1	0	6	0	0	0	0	0	0	0	83	509
05:20:00 PM	0	5	0	0	0	4	2	0	2	0	1	0	0	0	0	0	72	468
05:25:00 PM	0	12	0	0	0	4	1	0	4	0	1	0	0	0	0	0	63	446
05:30:00 PM	0	3	0	0	0	11	0	0	8	0	4	0	0	0	0	0	62	390
05:35:00 PM	0	4	0	0	0	9	1	0	4	0	2	0	0	0	0	0	68	339
05:40:00 PM	0	9	0	0	0	6	3	0	4	0	1	0	0	0	0	0	69	309
05:45:00 PM	0	10	0	0	0	6	1	0	3	0	2	0	0	0	0	0	65	289
05:50:00 PM	0	10	0	0	0	5	0	0	4	0	1	0	0	0	0	0	65	277
05:55:00 PM	0	6	0	0	0	7	0	0	2	0	0	0	0	0	0	0	57	275
06:00:00 PM	0	9	0	0	0	4	2	0	1	0	1	0	0	0	0	0	52	262
06:05:00 PM	0	10	0	0	0	9	2	0	2	0	3	0	0	0	0	0	58	263
06:10:00 PM	2	10	0	0	0	8	4	0	4	0	2	0	0	0	0	0	73	262
06:15:00 PM	0	8	0	0	0	8	1	0	6	0	3	0	0	0	0	0	82	261
06:20:00 PM	1	8	0	0	0	7	3	0	4	0	2	0	0	0	0	0	81	272
06:25:00 PM	0	9	0	0	0	5	3	0	2	0	1	0	0	0	0	0	71	270
06:30:00 PM	0	4	0	0	0	3	1	0	1	0	1	0	0	0	0	0	55	254
06:35:00 PM	0	5	0	0	0	5	2	0	1	0	0	0	0	0	0	0	43	247
06:40:00 PM	0	5	0	0	0	6	0	0	3	0	3	0	0	0	0	0	40	241
06:45:00 PM	0	5	0	0	0	6	1	0	0	0	0	0	0	0	0	0	42	231
06:50:00 PM	1	3	0	0	0	6	1	0	1	0	2	0	0	0	0	0	43	225
06:55:00 PM	0	8	0	0	0	3	1	0	2	0	0	0	0	0	0	0	40	224
07:00:00 PM	0	11	0	0	0	3	2	0	1	0	0	0	0	0	0	0	45	224
07:05:00 PM	0	5	0	0	0	3	1	0	4	0	1	0	0	0	0	0	45	212
07:10:00 PM	1	9	0	0	0	1	0	0	5	0	0	0	0	0	0	0	47	198
07:15:00 PM	0	3	0	0	0	3	0	0	1	0	0	0	0	0	0	0	37	179
07:20:00 PM	0	19	0	0	0	3	0	0	0	0	0	0	0	0	0	0	45	176
07:25:00 PM	0	13	0	0	0	5	0	0	2	0	0	0	0	0	0	0	49	176
07:30:00 PM	0	4	0	0	0	3	0	0	1	0	0	0	0	0	0	0	50	174
07:35:00 PM	0	5	0	0	0	2	0	0	0	0	1	0	0	0	0	0	36	169
07:40:00 PM	0	8	0	0	0	2	0	0	2	0	0	0	0	0	0	0	28	164
07:45:00 PM	0	15	0	0	0	6	0	0	1	0	0	0	0	0	0	0	42	174

# K-D-N

## KEY DATA NETWORK

07:50:00 PM	0	5	0	0	0	4	1	0	0	0	1	0	0	0	0	0	0	45	171
07:55:00 PM	0	6	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	40	164
08:00:00 PM	0	11	0	0	0	3	0	0	1	0	0	0	0	0	0	0	0	33	162
08:05:00 PM	0	4	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	30	156
08:10:00 PM	0	5	0	0	0	5	0	0	0	0	0	0	0	0	0	0	0	33	150
08:15:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	21	146
08:20:00 PM	0	1	0	0	0	1	0	0	2	0	1	0	0	0	0	0	0	18	129
08:25:00 PM	0	7	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	17	118
08:30:00 PM	0	4	0	0	0	2	1	0	1	0	0	0	0	0	0	0	0	22	118
08:35:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	19	112
08:40:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	11	101
08:45:00 PM	0	4	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	10	86
08:50:00 PM	1	2	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	15	82
08:55:00 PM	1	2	0	0	0	1	1	0	1	0	1	0	0	0	0	0	0	21	82
09:00:00 PM	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	16	69
09:05:00 PM	1	2	0	0	0	1	1	0	0	0	1	0	0	0	0	0	0	15	67
09:10:00 PM	0	3	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	15	64
09:15:00 PM	1	3	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	20	68
09:20:00 PM	1	4	0	0	0	4	2	0	1	0	0	0	0	0	0	0	0	26	75
09:25:00 PM	0	2	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	24	71
09:30:00 PM	1	4	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	27	73
09:35:00 PM	0	8	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	28	84
09:40:00 PM	0	4	0	0	0	3	0	0	1	0	1	0	0	0	0	0	0	32	92
09:45:00 PM	1	3	0	0	0	5	2	0	1	0	0	0	0	0	0	0	0	34	97
09 50:00 PM	3	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	26	95
09:55:00 PM	0	1	0	0	0	2	2	0	1	0	1	0	0	0	0	0	0	24	95





Data Provided by K-D-N.com 503-594-4224	
N/S street:	Laurel Ln
EW street:	I-84 EB Ramps
City, State	Boardman OR
Study ID #	
Location	45.839704 - -119.668397
Start Date	Tuesday, October 08, 2019
Start Time	06:00:00 AM
Peak Hour Start	03:45:00 PM
Peak 15 Min Start	04:25:00 PM
PHF (15-Min Int)	0.77

Peak-Hour Volumes (PHV)																							
Northbound				Southbound				Eastbound				Westbound				Entering				Leaving			
Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	NB	SB	EB	WB	NB	SB	EB	WB
0	25	54	0	238	34	0	0	20	0	8	0	0	0	0	0	79	272	28	0	42	45	0	292

Percent Heavy Vehicles																							
0.0%	12.0%	3.7%	0.0%	10.1%	11.8%	0.0%	0.0%	75.0%	0.0%	62.5%	0.0%	0.0%	0.0%	0.0%	0.0%	6.3%	10.3%	71.4%	0.0%	21.4%	40.0%	#DIV/0!	8.9%

PHV- Bicycles														PHV - Pedestrians							
Northbound				Southbound				Eastbound				Westbound				in Crosswalk					
Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Sum	NB	SB	EB	WB	Sum
0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

All Vehicle Volumes																		
Time	Northbound				Southbound				Eastbound				Westbound				15 Min	1 HR
	Laurel Ln				Laurel Ln				I-84 EB Ramps				I-84 EB Ramps					
	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Left	Thru	Right	Utum	Sum	Sum
06:00:00 AM	0	0	1	0	7	4	0	0	2	0	0	0	0	0	0	0		
06:05:00 AM	0	6	2	0	9	6	0	0	1	0	0	0	0	0	0	0		
06:10:00 AM	0	5	0	0	2	0	0	0	2	0	1	0	0	0	0	0	48	
06:15:00 AM	0	4	0	0	9	3	0	0	2	0	0	0	0	0	0	0	52	
06:20:00 AM	0	3	0	0	13	3	0	0	3	0	0	0	0	0	0	0	50	
06:25:00 AM	0	4	3	0	12	4	0	0	4	0	0	0	0	0	0	0	67	
06:30:00 AM	0	4	1	0	11	2	0	0	11	0	1	0	0	0	0	0	79	
06:35:00 AM	0	9	2	0	4	2	0	0	8	0	0	0	0	0	0	0	82	
06:40:00 AM	0	12	1	0	5	1	0	0	6	1	0	0	0	0	0	0	81	
06:45:00 AM	0	12	1	0	6	7	0	0	4	0	0	0	0	0	0	0	81	
06:50:00 AM	0	8	0	0	5	5	0	0	9	0	0	0	0	0	0	0	83	
06:55:00 AM	0	5	1	0	4	3	0	0	0	0	1	0	0	0	0	0	71	267
07:00:00 AM	0	8	1	0	10	2	0	0	3	0	0	0	0	0	0	0	65	277
07:05:00 AM	0	2	0	0	12	5	0	0	5	0	1	0	0	0	0	0	63	278
07:10:00 AM	0	6	4	0	12	2	0	0	4	0	0	0	0	0	0	0	77	296
07:15:00 AM	0	0	0	0	16	3	0	0	3	0	0	0	0	0	0	0	75	300
07:20:00 AM	0	3	3	0	12	12	0	0	2	0	0	0	0	0	0	0	82	310
07:25:00 AM	0	6	0	0	17	4	0	0	1	0	1	0	0	0	0	0	83	312
07:30:00 AM	0	4	3	0	22	4	0	0	3	0	1	0	0	0	0	0	98	319
07:35:00 AM	0	0	2	0	10	3	0	0	3	0	0	0	0	0	0	0	84	312
07:40:00 AM	0	3	2	0	10	4	0	0	4	0	0	0	0	0	0	0	78	309
07:45:00 AM	0	5	1	0	13	5	0	0	5	0	1	0	0	0	0	0	71	309
07:50:00 AM	0	1	2	0	7	2	0	0	5	0	1	0	0	0	0	0	71	300
07:55:00 AM	0	4	3	0	4	4	0	0	1	0	0	0	0	0	0	0	64	302
08:00:00 AM	0	3	2	0	10	1	0	0	2	0	2	0	0	0	0	0	54	298

# K-D-N

## KEY DATA NETWORK

08:05:00 AM	0	2	3	0	6	5	0	0	1	0	1	0	0	0	0	0	54	291
08:10:00 AM	0	3	1	0	7	5	0	0	2	0	0	0	0	0	0	0	56	281
08:15:00 AM	0	1	0	0	9	1	0	0	2	0	1	0	0	0	0	0	50	273
08:20:00 AM	0	3	3	0	7	5	0	0	3	0	2	0	0	0	0	0	55	264
08:25:00 AM	0	6	2	0	3	3	0	0	1	0	0	0	0	0	0	0	52	250
08:30:00 AM	0	5	2	0	8	5	0	0	1	1	1	0	0	0	0	0	61	236
08:35:00 AM	0	2	0	0	5	6	0	0	0	0	0	0	0	0	0	0	51	231
08:40:00 AM	0	3	0	0	9	4	0	0	1	0	0	0	0	0	0	0	53	225
08:45:00 AM	0	2	2	0	4	2	0	0	0	0	0	0	0	0	0	0	40	205
08:50:00 AM	0	2	2	0	5	5	0	0	1	0	1	0	0	0	0	0	43	203
08:55:00 AM	0	1	1	0	7	2	0	0	2	0	0	0	0	0	0	0	39	200
09:00:00 AM	0	1	2	0	5	1	0	0	2	0	0	0	0	0	0	0	40	191
09:05:00 AM	0	1	3	0	6	3	0	0	1	0	1	0	0	0	0	0	39	188
09:10:00 AM	0	1	3	0	9	2	0	0	3	0	1	0	0	0	0	0	45	189
09:15:00 AM	0	0	1	0	5	3	0	0	2	0	1	0	0	0	0	0	46	187
09:20:00 AM	0	3	2	0	3	3	0	0	3	0	0	0	0	0	0	0	45	178
09:25:00 AM	0	4	1	0	3	2	0	0	0	0	2	0	0	0	0	0	38	175
09:30:00 AM	0	2	2	0	8	5	0	0	3	0	0	0	0	0	0	0	46	172
09:35:00 AM	0	2	1	0	7	2	0	0	3	0	1	0	0	0	0	0	48	175
09:40:00 AM	0	2	0	0	12	1	0	0	1	0	0	0	0	0	0	0	52	174
09:45:00 AM	0	1	1	0	8	2	0	0	0	0	0	0	0	0	0	0	44	176
09:50:00 AM	0	1	2	0	2	1	0	0	2	0	2	0	0	0	0	0	38	170
09:55:00 AM	0	3	1	0	11	3	0	0	2	0	0	0	0	0	0	0	42	177
10:00:00 AM	0	1	1	0	6	1	0	0	2	0	1	0	0	0	0	0	42	178
10:05:00 AM	0	0	3	0	5	0	0	0	3	0	0	0	0	0	0	0	43	174
10:10:00 AM	0	0	0	0	6	1	0	0	2	0	0	0	0	0	0	0	32	164
10:15:00 AM	0	1	1	0	4	0	0	0	1	0	1	0	0	0	0	0	28	160
10:20:00 AM	0	2	2	0	5	2	0	0	4	0	0	0	0	0	0	0	32	161
10:25:00 AM	0	4	1	0	5	2	0	0	2	0	0	0	0	0	0	0	37	163
10:30:00 AM	0	0	2	0	5	4	0	0	4	0	0	0	0	0	0	0	44	158
10:35:00 AM	0	3	1	0	4	1	0	0	4	0	0	0	0	0	0	0	42	155
10:40:00 AM	0	4	0	0	5	0	0	0	0	0	1	0	0	0	0	0	38	149
10:45:00 AM	0	1	1	0	6	2	0	0	3	0	0	0	0	0	0	0	36	150
10:50:00 AM	0	4	1	0	6	6	0	0	1	0	0	0	0	0	0	0	41	158
10:55:00 AM	0	1	1	0	3	6	0	0	3	0	2	0	0	0	0	0	47	154
11:00:00 AM	0	2	2	0	7	2	0	0	7	1	2	0	0	0	0	0	57	165
11:05:00 AM	0	2	0	0	9	3	0	0	3	0	0	0	0	0	0	0	56	171
11:10:00 AM	0	4	1	0	11	2	0	0	2	0	0	0	0	0	0	0	60	182
11:15:00 AM	0	3	0	0	4	1	0	0	1	0	1	0	0	0	0	0	47	184
11:20:00 AM	0	2	0	0	2	1	0	0	1	0	0	0	0	0	0	0	36	175
11:25:00 AM	0	2	2	0	8	2	0	0	5	0	0	0	0	0	0	0	35	180
11:30:00 AM	0	3	2	0	4	3	0	0	6	0	2	0	0	0	0	0	45	185
11:35:00 AM	0	2	1	0	7	7	0	0	2	0	2	0	0	0	0	0	60	193
11:40:00 AM	0	3	1	0	8	0	0	0	5	0	1	0	0	0	0	0	59	201
11:45:00 AM	0	1	4	0	4	5	0	0	3	0	0	0	0	0	0	0	56	205
11:50:00 AM	0	2	2	0	2	2	0	0	4	0	1	0	0	0	0	0	48	200
11:55:00 AM	0	3	3	0	0	2	0	0	8	0	1	0	0	0	0	0	47	201

# K-D-N

## KEY DATA NETWORK

12:00:00 PM	0	6	1	0	5	1	0	0	1	0	0	0	0	0	0	0	0	44	192
12:05:00 PM	0	3	0	0	5	5	0	0	3	0	1	0	0	0	0	0	0	48	192
12:10:00 PM	0	2	1	0	3	2	0	0	7	1	0	0	0	0	0	0	0	47	188
12:15:00 PM	0	1	0	0	4	1	0	0	4	0	1	0	0	0	0	0	0	44	189
12:20:00 PM	0	2	2	0	5	5	0	0	2	0	1	0	0	0	0	0	0	44	200
12:25:00 PM	0	5	0	0	13	3	0	0	6	0	1	0	0	0	0	0	0	56	209
12:30:00 PM	0	2	2	0	4	4	0	0	6	0	0	0	0	0	0	0	0	63	207
12:35:00 PM	0	3	1	0	6	1	0	0	1	0	0	0	0	0	0	0	0	58	198
12:40:00 PM	0	1	3	0	7	3	0	0	6	0	2	0	0	0	0	0	0	52	202
12:45:00 PM	0	0	1	0	10	2	0	0	5	0	0	0	0	0	0	0	0	52	203
12:50:00 PM	0	4	1	0	5	1	0	0	1	0	0	0	0	0	0	0	0	52	202
12:55:00 PM	0	5	1	0	6	4	0	0	2	0	0	0	0	0	0	0	0	48	203
01:00:00 PM	0	2	0	0	7	6	0	0	2	0	1	0	0	0	0	0	0	48	207
01:05:00 PM	0	0	1	0	11	4	0	0	4	0	0	0	0	0	0	0	0	56	210
01:10:00 PM	0	1	0	0	8	2	0	0	1	1	1	0	0	0	0	0	0	52	208
01:15:00 PM	0	1	0	0	7	2	0	0	4	0	0	0	0	0	0	0	0	48	211
01:20:00 PM	0	2	1	0	8	0	0	0	5	0	1	0	0	0	0	0	0	45	211
01:25:00 PM	0	1	2	0	5	2	0	0	6	0	0	0	0	0	0	0	0	47	199
01:30:00 PM	0	1	2	0	4	2	0	0	6	0	1	0	0	0	0	0	0	49	197
01:35:00 PM	0	1	2	0	7	2	0	0	3	0	0	0	0	0	0	0	0	47	200
01:40:00 PM	0	3	0	0	9	5	0	0	4	0	0	0	0	0	0	0	0	52	199
01:45:00 PM	0	1	0	0	6	7	0	0	3	0	0	0	0	0	0	0	0	53	198
01:50:00 PM	0	1	0	0	4	2	0	0	3	0	1	0	0	0	0	0	0	49	197
01:55:00 PM	0	1	1	0	16	3	0	0	2	0	1	0	0	0	0	0	0	52	203
02:00:00 PM	0	3	1	0	8	4	0	0	2	0	0	0	0	0	0	0	0	53	203
02:05:00 PM	0	4	0	0	8	4	0	0	2	0	0	0	0	0	0	0	0	60	201
02:10:00 PM	0	1	4	0	9	2	0	0	0	0	0	0	0	0	0	0	0	52	203
02:15:00 PM	0	1	1	0	14	3	0	0	2	0	0	0	0	0	0	0	0	55	210
02:20:00 PM	0	1	0	0	12	5	0	0	4	0	1	0	0	0	0	0	0	60	216
02:25:00 PM	0	3	2	0	5	2	0	0	6	0	0	0	0	0	0	0	0	62	218
02:30:00 PM	0	2	2	0	7	8	0	0	1	1	0	0	0	0	0	0	0	62	223
02:35:00 PM	0	1	0	0	10	4	0	0	4	1	2	0	0	0	0	0	0	61	230
02:40:00 PM	0	5	3	0	10	5	0	0	3	0	0	0	0	0	0	0	0	69	235
02:45:00 PM	0	3	2	0	16	2	0	0	9	2	1	0	0	0	0	0	0	83	253
02:50:00 PM	0	5	1	0	12	3	0	0	6	0	0	0	0	0	0	0	0	88	269
02:55:00 PM	0	7	1	0	12	8	0	0	7	0	1	0	0	0	0	0	0	98	281
03:00:00 PM	0	3	2	0	15	4	0	0	0	0	1	0	0	0	0	0	0	88	288
03:05:00 PM	0	0	0	0	1	1	0	0	2	0	3	0	0	0	0	0	0	68	277
03:10:00 PM	0	2	5	0	21	7	0	0	2	0	4	0	0	0	0	0	0	73	302
03:15:00 PM	0	2	2	0	21	3	0	0	4	0	1	0	0	0	0	0	0	81	314
03:20:00 PM	0	5	3	0	18	0	0	0	1	0	2	0	0	0	0	0	0	103	320
03:25:00 PM	0	2	0	0	10	0	0	0	1	0	0	0	0	0	0	0	0	75	315
03:30:00 PM	0	1	2	0	22	5	0	0	1	0	0	0	0	0	0	0	0	73	325
03:35:00 PM	0	0	4	0	29	6	0	0	1	0	1	0	0	0	0	0	0	85	344
03:40:00 PM	0	2	1	0	17	2	0	0	1	0	3	0	0	0	0	0	0	98	344
03:45:00 PM	0	3	4	0	21	6	0	0	0	0	0	0	0	0	0	0	0	101	343
03:50:00 PM	0	2	1	0	18	3	0	0	2	0	0	0	0	0	0	0	0	86	342

# K-D-N

## KEY DATA NETWORK

03:55:00 PM	0	3	1	0	11	0	0	0	2	0	1	0	0	0	0	0	78	324
04:00:00 PM	0	0	1	0	11	6	0	0	1	0	0	0	0	0	0	0	63	318
04:05:00 PM	0	1	3	0	21	10	0	0	2	0	2	0	0	0	0	0	76	350
04:10:00 PM	0	0	1	0	30	1	0	0	1	0	0	0	0	0	0	0	91	342
04:15:00 PM	0	2	1	0	29	0	0	0	1	0	0	0	0	0	0	0	105	342
04:20:00 PM	0	1	1	0	17	0	0	0	1	0	0	0	0	0	0	0	86	333
04:25:00 PM	0	1	6	0	19	4	0	0	3	0	2	0	0	0	0	0	88	355
04:30:00 PM	0	0	22	0	18	2	0	0	2	0	2	0	0	0	0	0	101	370
04:35:00 PM	0	5	8	0	27	1	0	0	0	0	1	0	0	0	0	0	123	371
04:40:00 PM	0	7	5	0	16	1	0	0	5	0	0	0	0	0	0	0	122	379
04:45:00 PM	0	1	3	0	16	3	0	0	5	0	4	0	0	0	0	0	108	377
04:50:00 PM	0	2	1	0	13	2	0	0	3	1	0	0	0	0	0	0	88	373
04:55:00 PM	0	3	3	0	10	1	0	0	1	0	2	0	0	0	0	0	74	375
05:00:00 PM	0	2	1	0	12	5	0	0	0	0	0	0	0	0	0	0	62	376
05:05:00 PM	0	0	2	0	23	6	0	0	0	0	1	0	0	0	0	0	72	369
05:10:00 PM	0	5	7	0	6	5	0	0	3	0	1	0	0	0	0	0	79	363
05:15:00 PM	0	3	3	0	9	4	0	0	0	0	0	0	0	0	0	0	78	349
05:20:00 PM	0	2	1	0	7	2	0	0	5	0	0	0	0	0	0	0	63	346
05:25:00 PM	0	2	0	0	8	0	0	0	0	0	0	0	0	0	0	0	46	321
05:30:00 PM	0	2	5	0	11	2	0	0	0	0	0	0	0	0	0	0	47	295
05:35:00 PM	0	1	0	0	12	6	0	0	1	0	1	0	0	0	0	0	51	274
05:40:00 PM	0	0	3	0	12	7	0	0	2	0	2	0	0	0	0	0	67	266
05:45:00 PM	0	5	0	0	6	1	0	0	0	0	1	0	0	0	0	0	60	247
05:50:00 PM	0	2	2	0	5	2	0	0	2	0	1	0	0	0	0	0	53	239
05:55:00 PM	0	0	1	0	9	6	0	0	1	0	0	0	0	0	0	0	44	236
06:00:00 PM	0	2	3	0	12	5	0	0	1	0	0	0	0	0	0	0	54	239
06:05:00 PM	0	2	2	0	7	3	0	0	1	0	0	0	0	0	0	0	55	222
06:10:00 PM	0	2	0	0	9	2	0	0	2	0	1	0	0	0	0	0	54	211
06:15:00 PM	0	2	2	0	11	4	0	0	2	0	1	0	0	0	0	0	53	214
06:20:00 PM	0	1	1	0	3	3	0	0	5	0	0	0	0	0	0	0	51	210
06:25:00 PM	0	2	0	0	10	2	0	0	0	0	2	0	0	0	0	0	51	216
06:30:00 PM	0	1	1	0	12	2	0	0	1	0	1	0	0	0	0	0	47	214
06:35:00 PM	0	0	1	0	6	2	0	0	2	0	0	0	0	0	0	0	45	204
06:40:00 PM	0	3	1	0	7	2	0	0	1	0	0	0	0	0	0	0	43	192
06:45:00 PM	0	2	1	0	8	3	0	0	2	0	1	0	0	0	0	0	42	196
06:50:00 PM	0	1	0	0	9	0	0	0	1	0	0	0	0	0	0	0	42	193
06:55:00 PM	0	2	1	0	8	0	0	0	3	0	0	0	0	0	0	0	42	190
07:00:00 PM	0	1	0	0	9	2	0	0	0	0	0	0	0	0	0	0	37	179
07:05:00 PM	0	1	1	0	13	3	0	0	0	0	1	0	0	0	0	0	45	183
07:10:00 PM	0	2	0	0	9	4	0	0	4	0	0	0	0	0	0	0	50	186
07:15:00 PM	0	3	2	0	20	2	0	0	0	0	1	0	0	0	0	0	66	192
07:20:00 PM	0	0	0	0	21	2	0	0	2	0	0	0	0	0	0	0	72	204
07:25:00 PM	0	1	1	0	6	2	0	0	0	0	1	0	0	0	0	0	64	199
07:30:00 PM	0	0	0	0	10	1	0	0	0	0	1	0	0	0	0	0	48	193
07:35:00 PM	0	0	1	0	7	1	0	0	0	0	0	0	0	0	0	0	32	191
07:40:00 PM	0	1	1	0	4	2	0	0	0	1	2	0	0	0	0	0	32	188
07:45:00 PM	0	1	0	0	4	1	0	0	2	0	0	0	0	0	0	0	28	179



# K-D-N

## KEY DATA NETWORK

07:50:00 PM	0	3	1	0	1	2	0	0	0	0	0	1	0	0	0	0	0	27	176
07:55:00 PM	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	18	164
08:00:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	152
08:05:00 PM	0	0	0	0	2	2	0	0	0	2	0	1	0	0	0	0	0	9	140
08:10:00 PM	0	1	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	11	125
08:15:00 PM	0	1	2	0	0	2	0	0	0	1	0	1	0	0	0	0	0	18	104
08:20:00 PM	0	3	4	0	1	1	0	0	0	0	0	0	0	0	0	0	0	20	88
08:25:00 PM	0	0	0	0	3	3	0	0	0	0	0	0	0	0	0	0	0	22	83
08:30:00 PM	0	0	1	0	2	4	0	0	0	3	0	1	0	0	0	0	0	26	82
08:35:00 PM	0	1	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	22	78
08:40:00 PM	0	1	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	21	72
08:45:00 PM	0	0	1	0	2	2	0	0	0	0	0	0	0	0	0	0	0	15	69
08:50:00 PM	0	0	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	13	64
08:55:00 PM	0	0	0	0	2	1	0	0	0	3	0	0	0	0	0	0	0	14	68
09:00:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	10	69
09:05:00 PM	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	9	64
09:10:00 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	5	62
09:15:00 PM	0	2	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	9	60
09:20:00 PM	0	0	0	0	0	1	0	0	0	4	0	0	0	0	0	0	0	12	56
09:25:00 PM	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	13	53
09:30:00 PM	0	1	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	11	45
09:35:00 PM	0	1	0	0	1	0	0	0	0	1	0	1	0	0	0	0	0	10	44
09:40:00 PM	0	1	1	0	6	1	0	0	0	2	0	0	0	0	0	0	0	18	50
09:45:00 PM	0	0	1	0	6	3	0	0	0	1	0	0	0	0	0	0	0	26	56
09:50:00 PM	0	0	0	0	1	1	0	0	0	1	0	0	0	0	0	0	0	25	56
09:55:00 PM	0	1	0	0	4	3	0	0	0	0	0	0	0	0	0	0	0	22	58



# K-D-N

KEY DATA NETWORK

Data Provided by K-D-N.com 503-594-4224	
N/S street:	Laurel Ln
EW street:	I-84 WB Ramps
City, State	Boardman OR
Study ID #	
Location	45.841505 -119.668378
Start Date	Tuesday, October 08, 2019
Start Time	06:00:00 AM
Peak Hour Start	06:35:00 AM
Peak 15 Min Start	06:45:00 AM
PHF (15-Min Int)	0.72

Peak-Hour Volumes (PHV)																							
Northbound				Southbound				Eastbound				Westbound				Entering			Leaving				
Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	NB	SB	EB	WB	NB	SB	EB	WB
15	120	0	0	0	151	15	0	0	0	0	0	22	0	325	0	135	166	0	347	173	445	30	0

Percent Heavy Vehicles																							
40.0%	13.3%	0.0%	0.0%	0.0%	31.1%	26.7%	0.0%	0.0%	0.0%	0.0%	0.0%	9.1%	0.0%	10.2%	0.0%	16.3%	30.7%	#DIV/0!	10.1%	28.3%	11.0%	33.3%	0.0%

PHV - Bicycles														PHV - Pedestrians							
Northbound				Southbound				Eastbound				Westbound				in Crosswalk					
Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Sum	NB	SB	EB	WB	Sum
0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

All Vehicle Volumes																		
Time	Northbound				Southbound				Eastbound				Westbound				15 Min	1 HR
	Laurel Ln				Laurel Ln				I-84 WB Ramps				I-84 WB Ramps					
	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Left	Thru	Right	Uturn	Sum	Sum
06:00:00 AM	1	7	0	0	0	7	0	0	0	0	0	0	3	0	22	0		
06:05:00 AM	0	4	0	0	0	7	1	0	0	0	0	0	4	0	20	0		
06:10:00 AM	2	3	0	0	0	8	5	0	0	0	0	0	2	0	14	0	110	
06:15:00 AM	0	12	0	0	0	5	0	0	0	0	0	0	1	0	10	0	98	
06:20:00 AM	2	8	0	0	0	10	3	0	0	0	0	0	2	0	21	0	108	
06:25:00 AM	2	5	0	0	0	16	0	0	0	0	0	0	2	0	20	0	119	
06:30:00 AM	2	5	0	0	0	9	0	0	0	0	0	0	2	0	30	0	139	
06:35:00 AM	1	14	0	0	0	13	0	0	0	0	0	0	1	0	25	0	147	
06:40:00 AM	0	19	0	0	0	4	1	0	0	0	0	0	2	0	36	0	164	
06:45:00 AM	1	17	0	0	0	3	0	0	0	0	0	0	1	0	40	0	178	
06:50:00 AM	1	17	0	0	0	8	2	0	0	0	0	0	5	0	47	0	204	
06:55:00 AM	1	16	0	0	0	9	2	0	0	0	0	0	3	0	51	0	224	
07:00:00 AM	0	5	0	0	0	3	2	0	0	0	0	0	2	0	28	0	202	
07:05:00 AM	2	11	0	0	0	11	1	0	0	0	0	0	2	0	15	0	164	
07:10:00 AM	2	4	0	0	0	17	1	0	0	0	0	0	1	0	20	0	127	
07:15:00 AM	3	7	0	0	0	19	3	0	0	0	0	0	0	0	9	0	128	
07:20:00 AM	0	3	0	0	0	15	2	0	0	0	0	0	1	0	22	0	129	
07:25:00 AM	2	4	0	0	0	26	0	0	0	0	0	0	0	0	16	0	132	
07:30:00 AM	2	3	0	0	0	23	1	0	0	0	0	0	4	0	16	0	140	
07:35:00 AM	3	4	0	0	0	20	3	0	0	0	0	0	0	0	16	0	143	
07:40:00 AM	0	4	0	0	0	10	2	0	0	0	0	0	3	0	17	0	131	
07:45:00 AM	0	7	0	0	0	13	2	0	0	0	0	0	2	0	20	0	126	
07:50:00 AM	2	8	0	0	0	15	2	0	0	0	0	0	3	1	28	0	139	
07:55:00 AM	1	6	0	0	0	7	1	0	0	0	0	0	0	0	17	0	135	
08:00:00 AM	0	3	0	0	0	6	0	0	0	0	0	0	3	0	17	0	120	



KEY DATA NETWORK

08:05:00 AM	1	4	0	0	0	10	0	0	0	0	0	0	0	3	0	9	0	88	499
08:10:00 AM	0	4	0	0	0	7	4	0	0	0	0	0	0	1	0	7	0	79	477
08:15:00 AM	2	2	0	0	0	10	2	0	0	0	0	0	0	1	0	16	0	83	469
08:20:00 AM	1	3	0	0	0	12	0	0	0	0	0	0	0	1	0	12	0	85	455
08:25:00 AM	1	5	0	0	0	7	2	0	0	0	0	0	0	3	0	8	0	88	433
08:30:00 AM	1	6	0	0	0	6	1	0	0	0	0	0	0	2	0	10	0	81	410
08:35:00 AM	1	4	0	0	0	10	1	0	0	0	0	0	0	2	0	7	0	77	389
08:40:00 AM	0	3	0	0	0	8	1	0	0	0	0	0	0	4	0	3	0	70	372
08:45:00 AM	2	1	0	0	0	11	0	0	0	0	0	0	0	2	0	10	0	70	354
08:50:00 AM	0	2	0	0	0	5	1	0	0	0	0	0	0	1	0	7	0	61	311
08:55:00 AM	0	3	0	0	0	6	2	0	0	0	0	0	0	2	0	11	0	66	303
09:00:00 AM	0	3	0	0	0	8	1	0	0	0	0	0	0	1	0	10	0	63	297
09:05:00 AM	1	2	0	0	0	4	2	0	0	0	0	0	0	1	0	6	0	63	286
09:10:00 AM	1	1	0	0	0	11	5	0	0	0	0	0	0	2	0	5	0	64	288
09:15:00 AM	1	3	0	0	0	7	2	0	0	0	0	0	0	1	0	7	0	62	276
09:20:00 AM	0	2	0	0	0	7	1	0	0	0	0	0	0	2	0	5	0	63	264
09:25:00 AM	1	5	0	0	0	4	0	0	0	0	0	0	0	2	0	11	0	61	261
09:30:00 AM	2	3	0	0	0	5	0	0	0	0	0	0	0	0	0	6	0	56	251
09:35:00 AM	1	3	0	0	0	12	1	0	0	0	0	0	0	1	0	4	0	61	248
09:40:00 AM	0	6	0	0	0	8	1	0	0	0	0	0	0	1	1	6	0	61	252
09:45:00 AM	0	2	0	0	0	12	1	0	0	0	0	0	0	1	0	8	0	69	250
09:50:00 AM	1	0	0	0	0	9	6	0	0	0	0	0	0	1	0	11	0	75	262
09:55:00 AM	0	3	0	0	0	5	3	0	0	0	0	0	0	1	0	3	0	67	253
10:00:00 AM	0	5	0	0	0	9	2	0	0	0	0	0	0	2	0	8	0	69	256
10:05:00 AM	1	2	0	0	0	5	2	0	0	0	0	0	0	0	0	11	0	62	261
10:10:00 AM	1	2	0	0	0	8	1	0	0	0	0	0	0	0	0	6	0	65	254
10:15:00 AM	0	2	0	0	0	4	3	0	0	0	0	0	0	0	0	5	0	53	247
10:20:00 AM	0	2	0	0	0	5	2	0	0	0	0	0	0	1	0	1	0	43	241
10:25:00 AM	1	6	0	0	0	7	0	0	0	0	0	0	0	1	0	9	0	49	242
10:30:00 AM	4	7	0	0	0	3	2	0	0	0	0	0	0	0	0	6	0	57	248
10:35:00 AM	0	4	0	0	0	9	1	0	0	0	0	0	0	1	0	7	0	68	248
10:40:00 AM	0	6	0	0	0	4	0	0	0	0	0	0	0	0	0	11	0	65	246
10:45:00 AM	1	3	0	0	0	6	4	0	0	0	0	0	0	0	0	7	0	64	243
10:50:00 AM	0	4	0	0	0	7	4	0	0	0	0	0	0	3	0	7	0	67	240
10:55:00 AM	1	6	0	0	0	10	5	0	0	0	0	0	0	1	0	9	0	78	257
11:00:00 AM	0	2	0	0	0	9	1	0	0	0	0	0	0	1	0	10	0	80	254
11:05:00 AM	0	9	0	0	0	7	2	0	0	0	0	0	0	1	0	4	0	78	256
11:10:00 AM	0	5	0	0	0	12	2	0	0	0	0	0	0	4	0	5	0	74	266
11:15:00 AM	1	5	0	0	0	7	5	0	0	0	0	0	0	1	0	7	0	77	278
11:20:00 AM	4	0	0	0	0	5	5	0	0	0	0	0	0	0	0	5	0	73	286
11:25:00 AM	2	2	0	0	0	4	3	0	0	0	0	0	0	0	0	7	0	63	280
11:30:00 AM	0	7	0	0	0	9	3	0	0	0	0	0	0	0	0	9	0	65	286
11:35:00 AM	0	8	0	0	0	7	13	0	0	0	0	0	0	2	0	7	0	83	301
11:40:00 AM	2	3	0	0	0	10	5	0	0	0	0	0	0	3	0	6	0	94	309
11:45:00 AM	3	4	0	0	0	10	2	0	0	0	0	0	0	0	0	7	0	92	314
11:50:00 AM	0	6	0	0	0	5	3	0	0	0	0	0	0	2	0	7	0	78	312
11:55:00 AM	1	4	0	0	0	4	3	0	0	0	0	0	0	0	0	5	0	66	297



KEY DATA NETWORK

12:00:00 PM	2	8	0	0	0	3	3	0	0	0	0	0	0	0	0	2	0	58	292
12:05:00 PM	0	7	0	0	0	6	5	0	0	0	0	0	0	0	10	0	0	63	297
12:10:00 PM	2	4	0	0	0	6	1	0	0	0	0	0	4	0	10	0	0	73	296
12:15:00 PM	2	7	0	0	0	4	6	0	0	0	0	0	0	0	6	0	0	80	295
12:20:00 PM	2	5	0	0	0	6	0	0	0	0	0	0	0	0	12	0	0	77	301
12:25:00 PM	1	4	0	0	0	11	1	0	0	0	0	0	0	0	7	0	0	74	307
12:30:00 PM	0	8	0	0	0	14	4	0	0	0	0	0	0	0	4	0	0	79	309
12:35:00 PM	2	6	0	0	0	7	3	0	0	0	0	0	0	0	4	0	0	76	294
12:40:00 PM	0	4	0	0	0	6	4	0	0	0	0	0	2	0	7	0	0	75	288
12:45:00 PM	0	8	0	0	0	12	3	0	0	0	0	0	1	0	7	0	0	76	293
12:50:00 PM	0	4	0	0	0	9	1	0	0	0	0	0	1	0	11	0	0	80	296
12:55:00 PM	1	6	0	0	0	6	2	0	0	0	0	0	0	0	6	0	0	78	300
01:00:00 PM	0	5	0	0	0	7	4	0	0	0	0	0	3	0	8	0	0	74	309
01:05:00 PM	1	4	0	0	0	11	1	0	0	0	0	0	1	0	9	0	0	75	308
01:10:00 PM	0	4	0	0	0	13	3	0	0	0	0	0	1	0	3	0	0	78	305
01:15:00 PM	0	1	0	0	0	11	4	0	0	0	0	0	1	0	10	0	0	78	307
01:20:00 PM	1	4	0	0	0	8	4	0	0	0	0	0	1	0	5	0	0	74	305
01:25:00 PM	0	7	0	0	0	6	3	0	0	0	0	0	0	0	10	0	0	76	307
01:30:00 PM	0	8	0	0	0	7	1	0	0	0	0	0	1	0	6	0	0	72	300
01:35:00 PM	0	6	0	0	0	9	3	0	0	0	0	0	0	0	4	0	0	71	300
01:40:00 PM	0	4	0	0	0	7	1	0	0	0	0	0	1	0	3	0	0	61	293
01:45:00 PM	1	6	0	0	0	11	2	0	0	0	0	0	1	0	12	0	0	71	295
01:50:00 PM	1	3	0	0	0	8	0	0	0	0	0	0	4	1	11	0	0	77	297
01:55:00 PM	0	4	0	0	0	10	1	0	0	0	0	0	1	0	5	0	0	82	297
02:00:00 PM	1	2	0	0	0	12	0	0	0	0	0	0	3	0	8	0	0	75	296
02:05:00 PM	1	5	0	0	0	17	2	0	0	0	0	0	3	0	13	0	0	88	310
02:10:00 PM	1	5	0	0	0	9	1	0	0	0	0	0	2	0	6	0	0	91	310
02:15:00 PM	0	2	0	0	0	9	4	0	0	0	0	0	0	0	5	0	0	85	303
02:20:00 PM	0	2	0	0	0	18	2	0	0	0	0	0	2	0	10	0	0	78	314
02:25:00 PM	1	5	0	0	0	11	3	0	0	0	0	0	4	0	14	0	0	92	326
02:30:00 PM	0	9	0	0	0	7	3	0	0	0	0	0	1	1	10	0	0	103	334
02:35:00 PM	0	3	0	0	0	9	2	0	0	0	0	0	3	0	16	0	0	102	345
02:40:00 PM	0	4	0	0	0	16	1	0	0	0	0	0	1	0	14	0	0	100	365
02:45:00 PM	1	8	0	0	0	13	4	0	0	0	0	0	2	0	12	0	0	109	372
02:50:00 PM	0	12	0	0	0	17	4	0	0	0	0	0	2	0	14	0	0	125	393
02:55:00 PM	1	10	0	0	0	17	1	0	0	0	0	0	0	0	13	0	0	131	414
03:00:00 PM	1	13	0	0	0	13	2	0	0	0	0	0	3	0	12	0	0	135	432
03:05:00 PM	2	0	0	0	0	22	5	0	0	0	0	0	1	1	7	1	0	125	430
03:10:00 PM	1	1	0	0	0	42	2	0	0	0	0	0	0	0	10	0	0	139	462
03:15:00 PM	1	3	0	0	0	28	2	0	0	0	0	0	1	0	13	0	0	143	490
03:20:00 PM	0	6	0	0	0	18	4	0	0	0	0	0	2	0	10	0	0	144	496
03:25:00 PM	1	6	0	0	0	17	4	0	0	0	0	0	0	0	10	0	0	126	496
03:30:00 PM	1	1	0	0	0	12	3	0	0	0	0	0	2	0	7	0	0	104	491
03:35:00 PM	1	1	0	0	0	34	3	0	0	0	0	0	2	0	8	0	0	113	507
03:40:00 PM	0	1	0	0	0	23	3	0	0	0	0	0	2	0	10	0	0	114	510
03:45:00 PM	0	3	0	0	0	20	4	0	0	0	0	0	1	0	12	0	0	128	510
03:50:00 PM	0	3	0	0	0	26	0	0	0	0	0	0	1	0	8	0	0	117	499



KEY DATA NETWORK

03:55:00 PM	0	5	0	0	0	18	0	0	0	0	0	0	0	1	0	6	0	108	487
04:00:00 PM	1	3	0	0	0	12	2	0	0	0	0	0	0	0	0	7	0	93	468
04:05:00 PM	0	1	0	0	0	15	2	0	0	0	0	0	0	4	0	6	0	83	457
04:10:00 PM	1	2	0	0	0	28	5	0	0	0	0	0	0	2	0	5	0	96	444
04:15:00 PM	0	1	0	0	0	28	1	0	0	0	0	0	0	0	0	2	0	103	428
04:20:00 PM	1	2	0	0	0	33	6	0	0	0	0	0	0	0	0	3	0	120	433
04:25:00 PM	0	2	0	0	0	13	1	0	0	0	0	0	0	0	0	4	0	97	415
04:30:00 PM	0	5	0	0	0	21	0	0	0	0	0	0	0	2	0	6	0	99	423
04:35:00 PM	0	1	0	0	0	24	4	0	0	0	0	0	0	1	0	9	0	93	413
04:40:00 PM	1	4	0	0	0	24	4	0	0	0	0	0	0	1	0	7	0	114	415
04:45:00 PM	0	12	0	0	0	16	2	0	0	0	0	0	0	1	0	7	0	118	413
04:50:00 PM	0	7	0	0	0	17	3	0	0	0	0	0	0	0	0	5	0	111	407
04:55:00 PM	1	3	0	0	0	13	1	0	0	0	0	0	0	0	0	7	0	95	402
05:00:00 PM	1	3	0	0	0	13	2	0	0	0	0	0	0	1	0	8	0	85	405
05:05:00 PM	1	1	0	0	0	15	2	0	0	0	0	0	0	3	0	9	0	84	408
05:10:00 PM	0	0	0	0	0	27	1	0	0	0	0	0	0	1	1	12	0	101	407
05:15:00 PM	3	6	0	0	0	10	3	0	0	0	0	0	0	1	0	4	0	100	402
05:20:00 PM	0	2	0	0	0	11	3	0	0	0	0	0	0	1	0	2	0	88	376
05:25:00 PM	2	5	0	0	0	6	1	0	0	0	0	0	0	0	0	8	0	68	378
05:30:00 PM	2	0	0	0	0	12	3	0	0	0	0	0	0	0	0	2	0	60	363
05:35:00 PM	1	1	0	0	0	12	2	0	0	0	0	0	0	0	0	8	0	65	348
05:40:00 PM	1	1	0	0	0	18	0	0	0	0	0	0	0	5	0	7	0	75	339
05:45:00 PM	0	2	0	0	0	10	1	0	0	0	0	0	0	2	0	8	0	79	324
05:50:00 PM	2	3	0	0	0	7	3	0	0	0	0	0	0	0	0	5	0	75	312
05:55:00 PM	2	2	0	0	0	8	1	0	0	0	0	0	0	0	1	4	0	61	305
06:00:00 PM	0	2	0	0	0	9	2	0	0	0	0	0	0	4	0	3	0	58	297
06:05:00 PM	1	3	0	0	0	17	2	0	0	0	0	0	0	3	0	3	0	67	295
06:10:00 PM	0	2	0	0	0	10	7	0	0	0	0	0	0	0	0	5	0	73	277
06:15:00 PM	0	4	0	0	0	11	4	0	0	0	0	0	0	0	0	6	0	78	275
06:20:00 PM	3	3	0	0	0	13	3	0	0	0	0	0	0	1	0	11	0	83	290
06:25:00 PM	0	6	0	0	0	6	1	0	0	0	0	0	0	1	0	8	0	81	290
06:30:00 PM	1	2	0	0	0	9	1	0	0	0	0	0	0	0	0	12	0	81	296
06:35:00 PM	0	0	0	0	0	13	1	0	0	0	0	0	0	1	0	7	0	69	294
06:40:00 PM	0	2	0	0	0	6	3	0	0	0	0	0	0	3	0	8	0	69	284
06:45:00 PM	1	0	0	0	0	9	0	0	0	0	0	0	0	0	0	15	0	69	286
06:50:00 PM	0	2	0	0	0	9	1	0	0	0	0	0	0	2	0	15	0	76	295
06:55:00 PM	0	2	0	0	0	10	1	0	0	0	0	0	0	0	0	11	0	78	301
07:00:00 PM	2	6	0	0	0	8	0	0	0	0	0	0	0	0	0	4	0	73	301
07:05:00 PM	0	0	0	0	0	10	1	0	0	0	0	0	0	1	0	2	0	58	286
07:10:00 PM	0	0	0	0	0	14	2	0	0	0	0	0	0	2	0	5	0	57	285
07:15:00 PM	0	6	0	0	0	13	1	0	0	0	0	0	0	1	0	2	0	60	283
07:20:00 PM	1	0	0	0	0	21	1	0	0	0	0	0	0	1	0	3	0	73	276
07:25:00 PM	0	2	0	0	0	21	3	0	0	0	0	0	0	1	0	6	0	83	287
07:30:00 PM	0	0	0	0	0	7	1	0	0	0	0	0	0	1	0	4	0	73	275
07:35:00 PM	0	0	0	0	0	12	2	0	0	0	0	0	0	0	0	7	0	67	274
07:40:00 PM	0	0	0	0	0	6	2	0	0	0	0	0	0	0	0	7	0	49	267
07:45:00 PM	2	0	0	0	0	5	1	0	0	0	0	0	0	1	0	2	0	47	253

# K-D-N

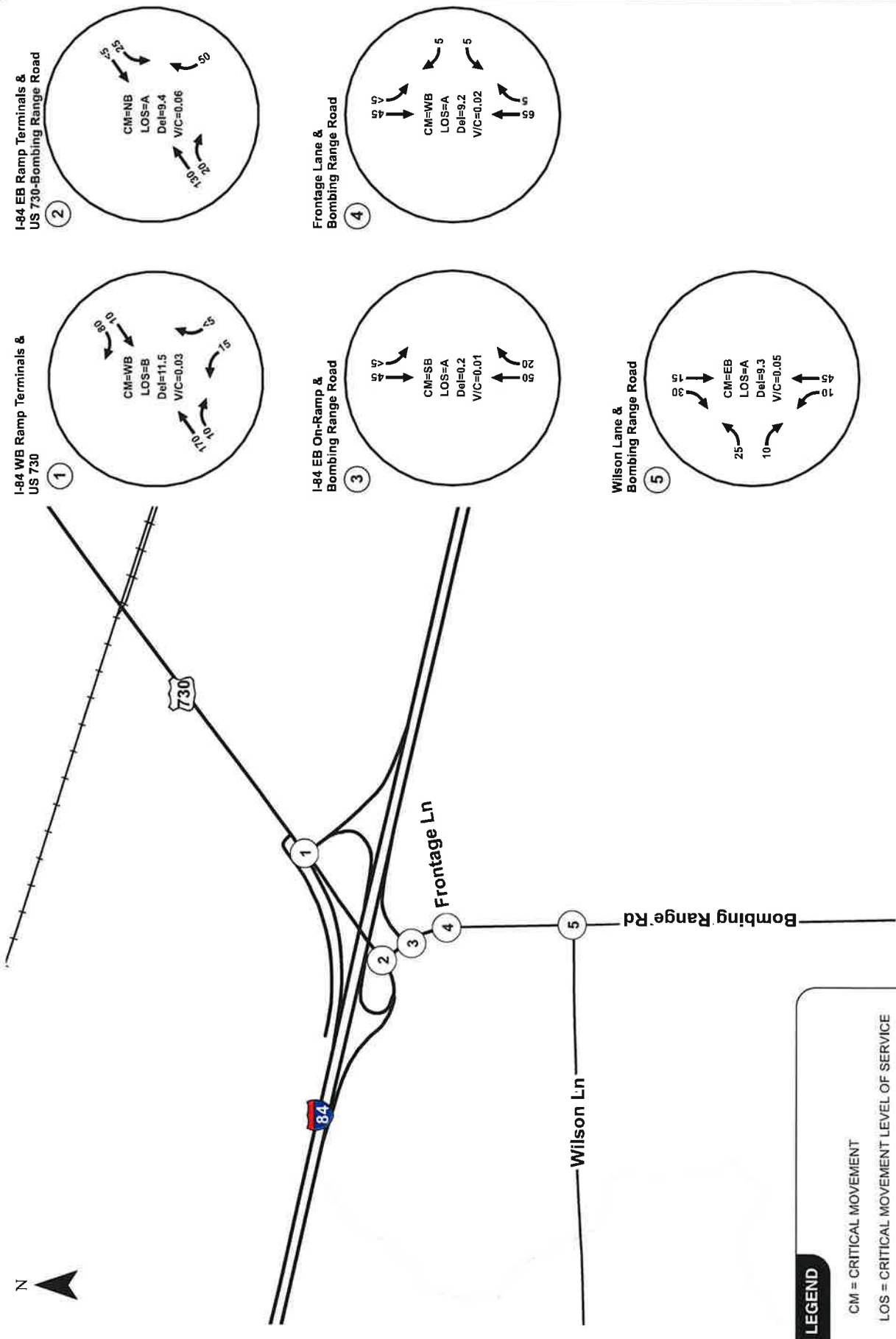
## KEY DATA NETWORK

07:50:00 PM	0	2	0	0	0	3	1	0	0	0	0	0	1	0	3	0	36	234
07:55:00 PM	0	0	0	0	0	4	1	0	0	0	0	0	2	0	8	0	36	225
08:00:00 PM	0	0	0	0	0	7	0	0	0	0	0	0	1	0	6	0	39	219
08:05:00 PM	1	0	0	0	0	3	1	0	0	0	0	0	0	0	5	0	39	215
08:10:00 PM	0	4	0	0	0	3	0	0	0	0	0	0	0	0	4	0	35	203
08:15:00 PM	0	0	0	0	0	4	2	0	0	0	0	0	0	0	2	0	29	188
08:20:00 PM	1	2	0	0	0	0	1	0	0	0	0	0	1	0	3	0	27	169
08:25:00 PM	0	0	0	0	0	2	1	0	0	0	0	0	0	0	2	0	21	141
08:30:00 PM	0	0	0	0	0	6	0	0	0	0	0	0	1	0	2	0	22	137
08:35:00 PM	0	4	0	0	0	5	0	0	0	0	0	0	0	0	3	0	26	128
08:40:00 PM	1	0	0	0	0	2	1	0	0	0	0	0	0	0	2	0	27	119
08:45:00 PM	1	0	0	0	0	4	0	0	0	0	0	0	0	0	1	0	24	114
08:50:00 PM	0	0	0	0	0	3	2	0	0	0	0	0	1	0	1	0	19	111
08:55:00 PM	0	2	0	0	0	2	1	0	0	0	0	0	0	0	3	0	21	104
09:00:00 PM	1	2	0	0	0	2	1	0	0	0	0	0	0	0	5	0	26	101
09:05:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	1	0	3	0	25	97
09:10:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	2	0	20	89
09:15:00 PM	0	4	0	0	0	0	0	0	0	0	0	0	0	0	2	0	15	87
09:20:00 PM	1	0	0	0	0	3	0	0	0	0	0	0	0	0	4	0	17	87
09:25:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	1	0	2	0	20	88
09:30:00 PM	0	0	0	0	0	0	2	0	0	0	0	0	1	0	8	0	25	90
09:35:00 PM	0	4	0	0	0	1	1	0	0	0	0	0	0	0	4	0	27	88
09:40:00 PM	0	2	0	0	0	4	0	0	0	0	0	0	0	0	7	0	34	95
09:45:00 PM	0	4	0	0	0	4	1	0	0	0	0	0	0	0	4	0	36	102
09:50:00 PM	0	2	0	0	0	12	0	0	0	0	0	0	0	0	4	0	44	113
09:55:00 PM	0	0	0	0	0	2	0	0	0	0	0	0	1	0	4	0	38	112
10:00:00 PM	0	2	0	0	0	5	1	0	0	0	0	0	3	0	4	0	40	116
10:05:00 PM	0	4	0	0	0	6	1	0	0	0	0	0	0	0	2	0	35	123
10:10:00 PM	0	0	0	0	0	3	1	0	0	0	0	0	1	0	6	0	39	131
10:15:00 PM	0	0	0	0	0	5	2	0	0	0	0	0	1	0	3	0	35	136
10:20:00 PM	0	0	0	0	0	4	1	0	0	0	0	0	0	0	9	0	36	142
10:25:00 PM	1	4	0	0	0	4	1	0	0	0	0	0	0	0	7	0	42	153
10:30:00 PM	0	2	0	0	0	1	0	0	0	0	0	0	0	0	6	0	40	151





H:\profile



**LEGEND**

- CM = CRITICAL MOVEMENT
- LOS = CRITICAL MOVEMENT LEVEL OF SERVICE
- Del = CRITICAL MOVEMENT CONTROL DELAY
- V/C = CRITICAL VOLUME-TO-CAPACITY RATIO

EXISTING TRAFFIC CONDITIONS  
 30TH HIGHEST HOUR  
 MORROW COUNTY, OREGON  
 FIGURE 4-11



connecting to Main Street in Boardman. This multi-use path also extends to the northeast to Irrigon, providing a non-motorized connection between the two cities.

## MARINE FACILITIES

The Port of Morrow is strategically located along the Columbia River. Goods can be shipped via barge west to Portland and Seattle or upriver to the Tri-Cities in Washington and Lewiston, Idaho. Goods barged from the Port can reach oceangoing freighters in Portland within 24 hours, accessing markets through the Pacific Ocean. According to the Port, Tidewater Terminal is the largest container terminal upriver of Portland (Reference 3).

## RAIL FACILITIES

The Union Pacific Mainline passes through the IMSA. Businesses in the Port of Morrow are able to ship their goods across the country via rail due to the nearby location of the Hinkle Railyard, which is the largest hump yard in the West. Connections at Hinkle provide shippers the ability to send goods north and south via rail.

## EXISTING TRAFFIC VOLUMES AND PEAK HOUR OPERATIONS

Manual intersection turning movement counts were obtained from ODOT at each of the study intersections to assess the operational performance and characteristics within the study area. These counts were conducted on mid-week days in April 2010. A description of the analysis conducted with this data is summarized in the following sections.

### *Intersection Volumes*

Turning movement counts at each intersection were recorded from 6:00 a.m. to 10:00 p.m. Separate peak hours for each interchange area are identified due to their different natures (e.g. the POM interchange serves primarily industrial traffic and the I-84/US 730 interchange serves regional commuter and through traffic) and the distance between them. The weekday p.m. peak hour in the POM interchange area occurs from 3:00-4:00 p.m. The turning movement volumes at each study intersection are balanced where appropriate during this hour to account for the differences in data collection. The existing unadjusted turning movement traffic counts are provided in the *Technical Appendix*.

### *Seasonal Adjustments*

Following the methodology outlined by ODOT's Analysis Procedures Manual (APM, Reference 4), a seasonal adjustment factor was applied to the traffic counts collected for the existing conditions

analysis in order to estimate 30<sup>th</sup> highest hour volumes. The exception to this is I-84, since its volumes are taken from automatic traffic recorder (ATR) #25-008, which is located nearby on I-84 west of US 730, during the peak month (July). In consultation with ODOT staff, ATR #30-002, located on US 730 northeast of the study area and east of Umatilla at milepost 193.70, was determined to have the most similar characteristics to US 730 within the study area. The seasonal adjustment factor for counts conducted on US 730 and local roadways within the study area during April is 1.21.

Figure 4-6 illustrates the 16-hour volume peaking characteristics of the I-84 through traffic. Figures 4-7 through 4-8 illustrate the 16-hour volume peaking characteristics of the I-84 ramps at the POM interchange. Figure 4-9 shows the same for Laurel Lane. The volumes shown in these figures have been seasonally adjusted.

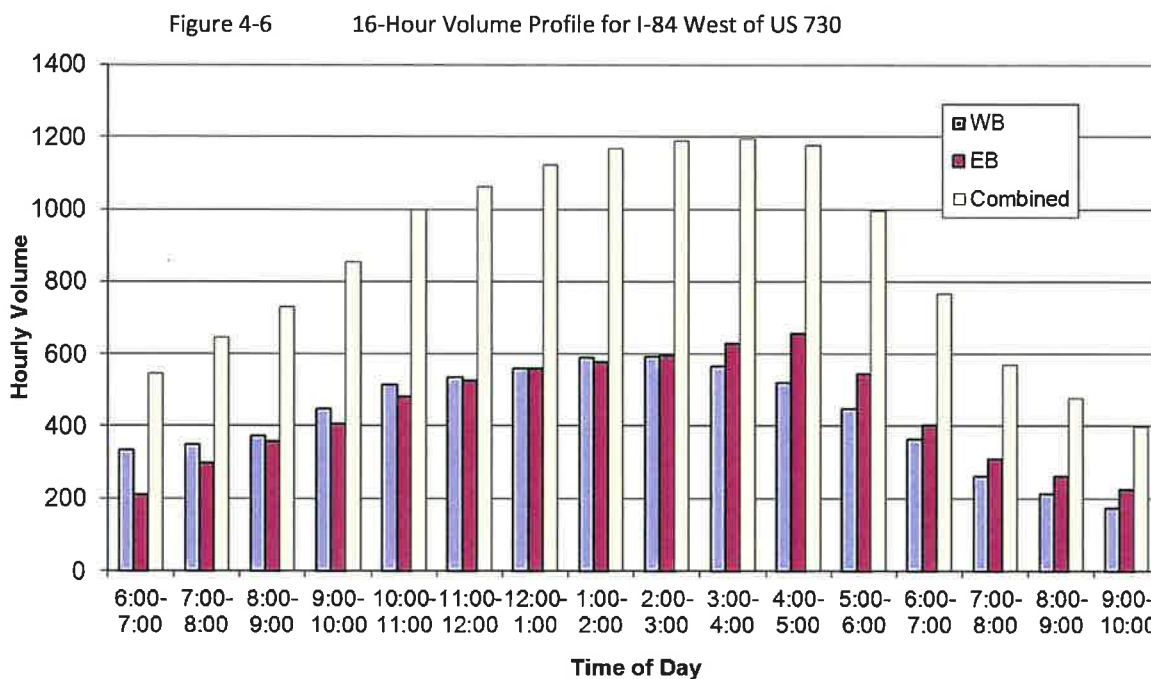


Figure 4-7 16-Hour Volume Profile for I-84 WB Ramps at Laurel Lane

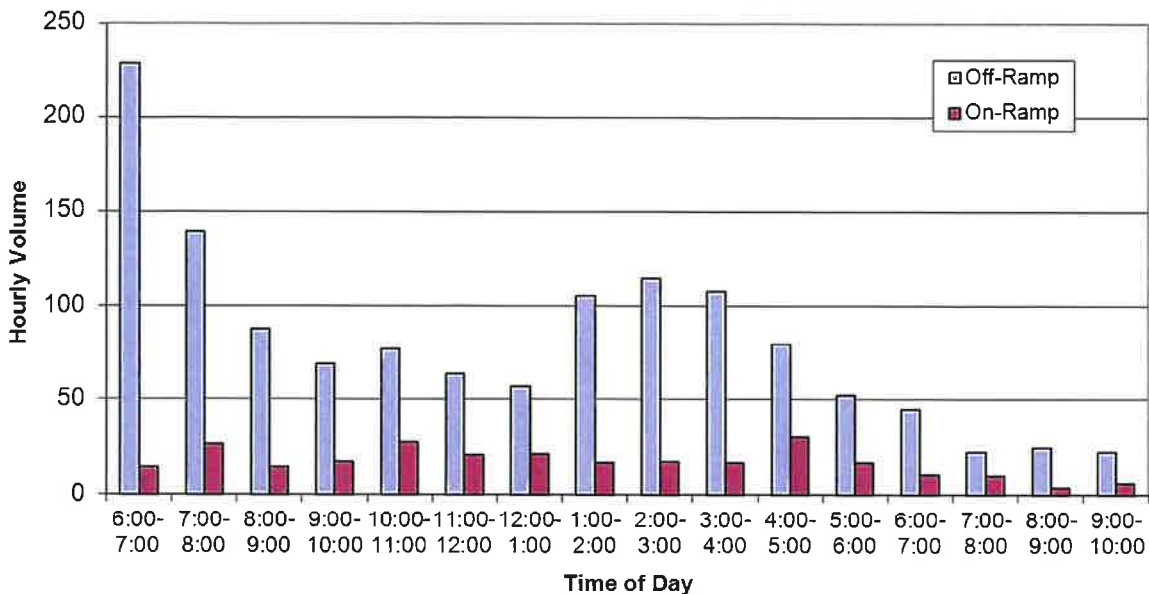
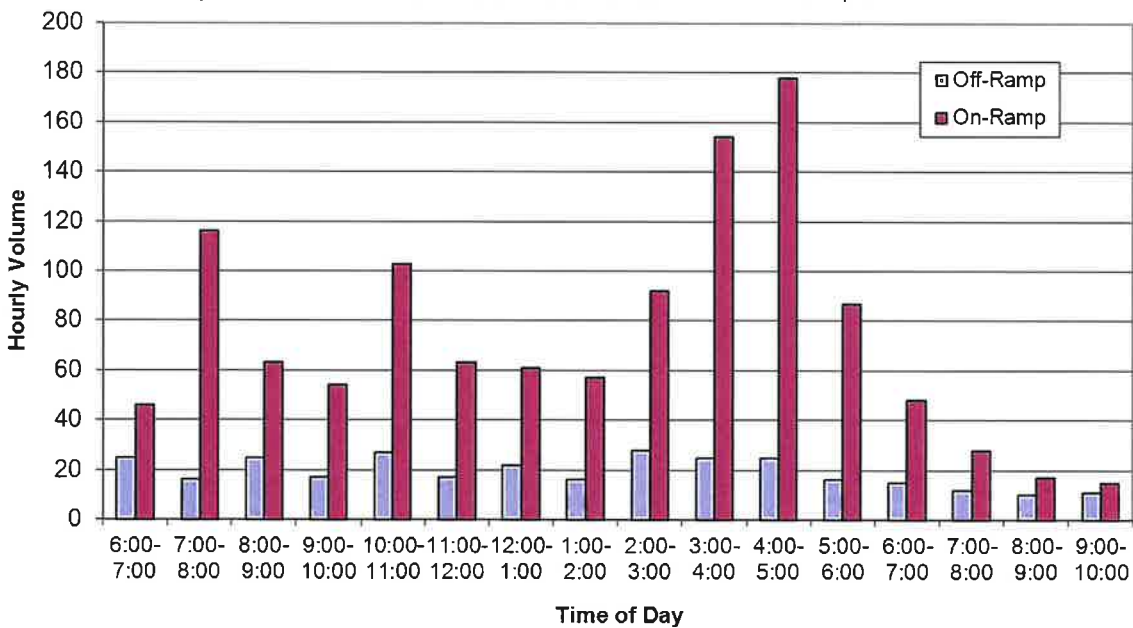
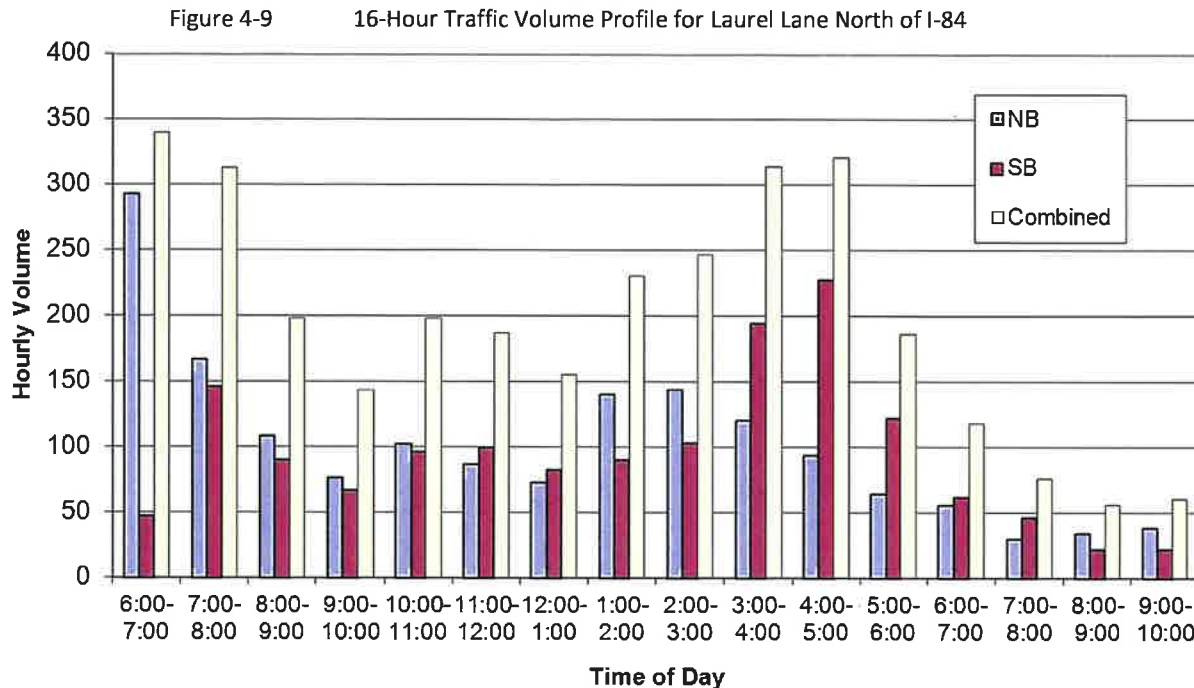


Figure 4-8 16-Hour Traffic Volume Profile for I-84 EB Ramps at Laurel Lane

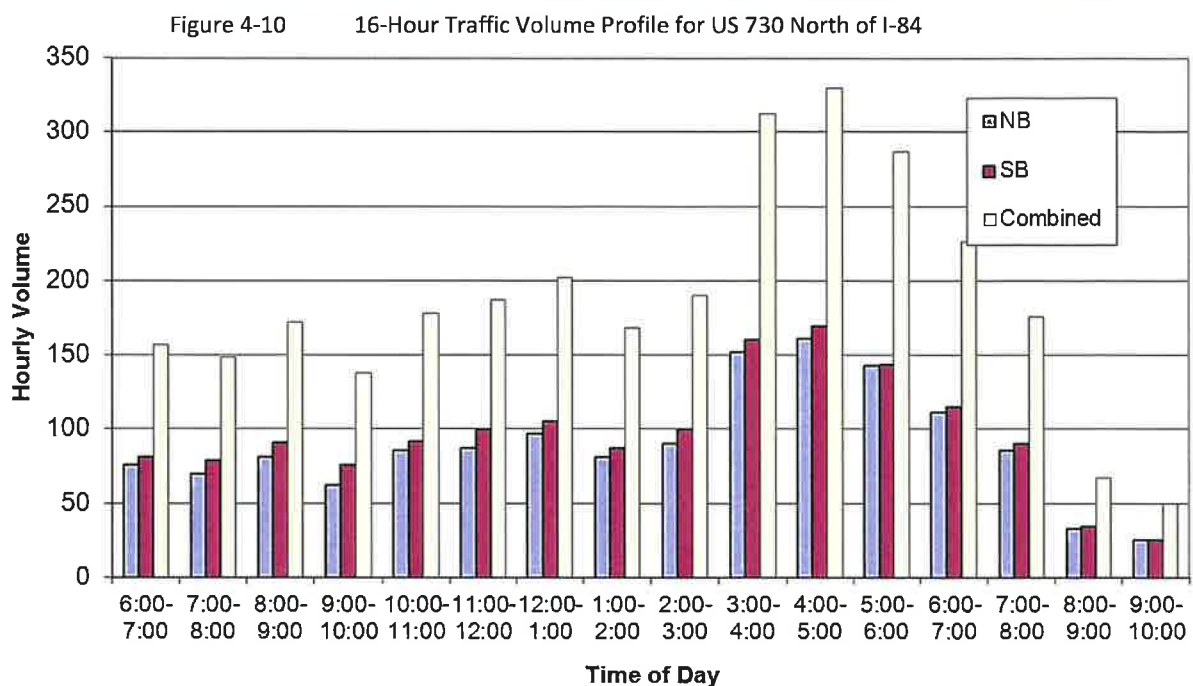




Traffic traveling to and from I-84 significantly influences traffic volumes on Laurel Lane. As the figures show, the peak hour volumes on each of the I-84 ramps are approximately 80% of the volumes on Laurel Lane during that same period. Essentially, most of the traffic on Laurel Lane in the proximity of I-84 is traveling to or from the interstate.

Figures 4-7 and 4-8 show that the I-84 ramps at the POM interchange each have a dominant traffic pattern that lasts throughout the day. Traffic volumes on the I-84 westbound off-ramp are significantly higher than volumes on the westbound on-ramp throughout the 16-hour period that counts were conducted. The exact reverse pattern occurs on the eastbound ramps, where the off-ramp has significantly lower volumes than the on-ramp. This indicates that much of the traffic utilizing this interchange is coming from and going to the east on I-84.

The weekday 30<sup>th</sup> highest hour intersection turning movement counts used for the existing conditions analysis are shown in Figure 4-10.



Traffic traveling to and from I-84 significantly influences traffic volumes on US 730. As the figures show, the peak hour volumes on each of the I-84 ramps are approximately 80% of the volumes on US 730 during that same period. Essentially, most of the traffic on US 730 in the proximity of I-84 is traveling to or from the interstate.

Figures 4-7 through 4-9 show that the I-84 ramps at the US 730 interchange each have a dominant traffic pattern that lasts throughout the day. Traffic volumes on the I-84 westbound on-ramp are significantly higher than volumes on the westbound off-ramp throughout the 16-hour period that counts were conducted. The exact reverse pattern occurs on the eastbound ramps, where the on-ramp has significantly lower volumes than the off-ramp. This indicates that much of the traffic utilizing this interchange is coming from and going to the west on I-84. This is not necessarily surprising given that I-82 is a faster connection to I-84 to the east of US 730 for traffic coming from or going to Washington or many of the destinations along US 730.

The weekday 30<sup>th</sup> highest hour intersection turning movement counts used for the existing conditions analysis are shown in Figure 4-11.





# Appendix B

## Synchro HCM 6 Reports





HCM 6th TWSC  
1: Bombing Range Rd/US 730 & I-84 WB On/Off Ramp

Data Center Due Dilligence  
2021 PM Peak Hour Existing Conditions

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations					↕			↕			↕	
Traffic Vol, veh/h	0	0	0	25	0	5	0	225	15	0	15	105
Future Vol, veh/h	0	0	0	25	0	5	0	225	15	0	15	105
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	1	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	27	0	5	0	245	16	0	16	114

Major/Minor	Minor1	Major1	Major2							
Conflicting Flow All	326	383	253	-	0	0	-	-	-	0
Stage 1	253	253	-	-	-	-	-	-	-	-
Stage 2	73	130	-	-	-	-	-	-	-	-
Critical Hdwy	6.42	6.52	6.22	-	-	-	-	-	-	-
Critical Hdwy Stg 1	5.42	5.52	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	5.42	5.52	-	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	-	-	-	-	-	-	-
Pot Cap-1 Maneuver	668	550	786	0	-	-	0	-	-	-
Stage 1	789	698	-	0	-	-	0	-	-	-
Stage 2	950	789	-	0	-	-	0	-	-	-
Platoon blocked, %				-	-	-	-	-	-	-
Mov Cap-1 Maneuver	668	0	786	-	-	-	-	-	-	-
Mov Cap-2 Maneuver	668	0	-	-	-	-	-	-	-	-
Stage 1	789	0	-	-	-	-	-	-	-	-
Stage 2	950	0	-	-	-	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.5	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT	SBR
Capacity (veh/h)	-	-	685	-
HCM Lane V/C Ratio	-	-	0.048	-
HCM Control Delay (s)	-	-	10.5	-
HCM Lane LOS	-	-	B	-
HCM 95th %tile Q(veh)	-	-	0.1	-

HCM 6th TWSC  
2: I-84 EB On/Off Ramp & Bombing Range Rd

Data Center Due Dilligence  
2021 PM Peak Hour Existing Conditions

Intersection						
Int Delay, s/veh	3					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations		↗	↘		↖	↗
Traffic Vol, veh/h	0	65	175	25	35	5
Future Vol, veh/h	0	65	175	25	35	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	400	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	71	190	27	38	5
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	-	204	0	0	217	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.22	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	0	837	-	-	1353	-
Stage 1	0	-	-	-	-	-
Stage 2	0	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	-	837	-	-	1353	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	NW	NE	SW			
HCM Control Delay, s	9.7	0	6.8			
HCM LOS	A					
Minor Lane/Major Mvmt	NET	NERNWLn1	SWL	SWT		
Capacity (veh/h)	-	-	837	1353	-	
HCM Lane V/C Ratio	-	-	0.084	0.028	-	
HCM Control Delay (s)	-	-	9.7	7.7	-	
HCM Lane LOS	-	-	A	A	-	
HCM 95th %tile Q(veh)	-	-	0.3	0.1	-	

HCM 6th TWSC  
4: Bombing Range Rd & Frontage Rd

Data Center Due Dilligence  
2021 PM Peak Hour Existing Conditions

Intersection						
Int Delay, s/veh	0.8					
Movement	NBT	NBR	SBL	SBT	SWL	SWR
Lane Configurations	↗			↖	↘	↙
Traffic Vol, veh/h	85	5	5	50	5	5
Future Vol, veh/h	85	5	5	50	5	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	92	5	5	54	5	5
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	97	0	159	95
Stage 1	-	-	-	-	95	-
Stage 2	-	-	-	-	64	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1496	-	832	962
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	959	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1496	-	830	962
Mov Cap-2 Maneuver	-	-	-	-	830	-
Stage 1	-	-	-	-	929	-
Stage 2	-	-	-	-	956	-
Approach	NB	SB	SW			
HCM Control Delay, s	0	0.7	9.1			
HCM LOS						A
Minor Lane/Major Mvmt	NBT	NBR	SBL	SBT/SWLn1		
Capacity (veh/h)	-	-	1496	-	891	
HCM Lane V/C Ratio	-	-	0.004	-	0.012	
HCM Control Delay (s)	-	-	7.4	0	9.1	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	-	0	

HCM 6th TWSC  
5: Bombing Range Rd & Wilson Lane

Data Center Due Dilligence  
2021 PM Peak Hour Existing Conditions

Intersection						
Int Delay, s/veh	3.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	35	15	15	55	20	35
Future Vol, veh/h	35	15	15	55	20	35
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	38	16	16	60	22	38

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	133	41	60	0	-
Stage 1	41	-	-	-	-
Stage 2	92	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	861	1030	1544	-	-
Stage 1	981	-	-	-	-
Stage 2	932	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	852	1030	1544	-	-
Mov Cap-2 Maneuver	852	-	-	-	-
Stage 1	970	-	-	-	-
Stage 2	932	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.3	1.6	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1544	-	899	-	-
HCM Lane V/C Ratio	0.011	-	0.06	-	-
HCM Control Delay (s)	7.4	0	9.3	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-

HCM 6th TWSC  
1: Bombing Range Rd/US 730 & I-84 WB On/Off Ramp

Data Center Due Dilligence  
2024 PM Peak Hour No Build Conditions

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations					↕			↗			↖	
Traffic Vol, veh/h	0	0	0	25	0	5	0	250	15	0	20	115
Future Vol, veh/h	0	0	0	25	0	5	0	250	15	0	20	115
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	1	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	27	0	5	0	272	16	0	22	125
Major/Minor	Minor1			Major1			Major2					
Conflicting Flow All				365	427	280	-	0	0	-	-	0
Stage 1				280	280	-	-	-	-	-	-	-
Stage 2				85	147	-	-	-	-	-	-	-
Critical Hdwy				6.42	6.52	6.22	-	-	-	-	-	-
Critical Hdwy Stg 1				5.42	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2				5.42	5.52	-	-	-	-	-	-	-
Follow-up Hdwy				3.518	4.018	3.318	-	-	-	-	-	-
Pot Cap-1 Maneuver				635	520	759	0	-	-	0	-	-
Stage 1				767	679	-	0	-	-	0	-	-
Stage 2				938	775	-	0	-	-	0	-	-
Platoon blocked, %												
Mov Cap-1 Maneuver				635	0	759	-	-	-	-	-	-
Mov Cap-2 Maneuver				635	0	-	-	-	-	-	-	-
Stage 1				767	0	-	-	-	-	-	-	-
Stage 2				938	0	-	-	-	-	-	-	-
Approach	WB			NB			SB					
HCM Control Delay, s				10.8			0			0		
HCM LOS				B								
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT	SBR								
Capacity (veh/h)	-	-	653	-	-							
HCM Lane V/C Ratio	-	-	0.05	-	-							
HCM Control Delay (s)	-	-	10.8	-	-							
HCM Lane LOS	-	-	B	-	-							
HCM 95th %tile Q(veh)	-	-	0.2	-	-							

HCM 6th TWSC  
2: I-84 EB On/Off Ramp & Bombing Range Rd

Data Center Due Dilligence  
2024 PM Peak Hour No Build Conditions

Intersection						
Int Delay, s/veh	3					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations		↗	↘		↖	↗
Traffic Vol, veh/h	0	70	195	30	40	5
Future Vol, veh/h	0	70	195	30	40	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	400	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	76	212	33	43	5
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	-	229	0	0	245	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.22	-	-	4.12	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	0	810	-	-	1321	-
Stage 1	0	-	-	-	-	-
Stage 2	0	-	-	-	-	-
Platoon blocked, %						
Mov Cap-1 Maneuver	-	810	-	-	1321	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Approach	NW	NE	SW			
HCM Control Delay, s	9.9	0	6.9			
HCM LOS	A					
Minor Lane/Major Mvmt	NET	NERNWLn1	SWL	SWT		
Capacity (veh/h)	-	-	810	1321	-	
HCM Lane V/C Ratio	-	-	0.094	0.033	-	
HCM Control Delay (s)	-	-	9.9	7.8	-	
HCM Lane LOS	-	-	A	A	-	
HCM 95th %tile Q(veh)	-	-	0.3	0.1	-	



HCM 6th TWSC  
4: Bombing Range Rd & Frontage Rd

Data Center Due Dilligence  
2024 PM Peak Hour No Build Conditions

Intersection						
Int Delay, s/veh	0.7					
Movement	NBT	NBR	SBL	SBT	SWL	SWR
Lane Configurations	↖			↗	↘	
Traffic Vol, veh/h	95	5	5	60	5	5
Future Vol, veh/h	95	5	5	60	5	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	103	5	5	65	5	5
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	108	0	181	106
Stage 1	-	-	-	-	106	-
Stage 2	-	-	-	-	75	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1483	-	808	948
Stage 1	-	-	-	-	918	-
Stage 2	-	-	-	-	948	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1483	-	806	948
Mov Cap-2 Maneuver	-	-	-	-	806	-
Stage 1	-	-	-	-	918	-
Stage 2	-	-	-	-	945	-
Approach	NB	SB	SW			
HCM Control Delay, s	0	0.6	9.2			
HCM LOS			A			
Minor Lane/Major Mvmt	NBT	NBR	SBL	SBT	SWLn1	
Capacity (veh/h)	-	-	1483	-	871	
HCM Lane V/C Ratio	-	-	0.004	-	0.012	
HCM Control Delay (s)	-	-	7.4	0	9.2	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	-	0	

HCM 6th TWSC  
5: Bombing Range Rd & Wilson Lane

Data Center Due Dilligence  
2024 PM Peak Hour No Build Conditions

Intersection						
Int Delay, s/veh	3.2					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	40	15	15	60	20	45
Future Vol, veh/h	40	15	15	60	20	45
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	43	16	16	65	22	49
Major/Minor	Minor2	Major1		Major2		
Conflicting Flow All	144	47	71	0	-	0
Stage 1	47	-	-	-	-	-
Stage 2	97	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	849	1022	1529	-	-	-
Stage 1	975	-	-	-	-	-
Stage 2	927	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	840	1022	1529	-	-	-
Mov Cap-2 Maneuver	840	-	-	-	-	-
Stage 1	964	-	-	-	-	-
Stage 2	927	-	-	-	-	-
Approach	EB	NB		SB		
HCM Control Delay, s	9.4	1.5		0		
HCM LOS	A					
Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR	
Capacity (veh/h)	1529	-	883	-	-	
HCM Lane V/C Ratio	0.011	-	0.068	-	-	
HCM Control Delay (s)	7.4	0	9.4	-	-	
HCM Lane LOS	A	A	A	-	-	
HCM 95th %tile Q(veh)	0	-	0.2	-	-	

HCM 6th TWSC  
1: Bombing Range Rd/US 730 & I-84 WB On/Off Ramp

Data Center Due Dilligence  
2024 PM Peak Hour Build Conditions

Intersection												
Int Delay, s/veh	0.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations				↕			↗			↘		
Traffic Vol, veh/h	0	0	0	29	0	5	0	271	39	0	26	115
Future Vol, veh/h	0	0	0	29	0	5	0	271	39	0	26	115
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	1	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	32	0	5	0	295	42	0	28	125
Major/Minor	Minor1			Major1			Major2					
Conflicting Flow All	407			469			316			-		
Stage 1	316			316			-			-		
Stage 2	91			153			-			-		
Critical Hdwy	6.42			6.52			6.22			-		
Critical Hdwy Stg 1	5.42			5.52			-			-		
Critical Hdwy Stg 2	5.42			5.52			-			-		
Follow-up Hdwy	3.518			4.018			3.318			-		
Pot Cap-1 Maneuver	600			492			724			0		
Stage 1	739			655			-			0		
Stage 2	933			771			-			0		
Platoon blocked, %	-			-			-			-		
Mov Cap-1 Maneuver	600			0			724			-		
Mov Cap-2 Maneuver	600			0			-			-		
Stage 1	739			0			-			-		
Stage 2	933			0			-			-		
Approach	WB			NB			SB					
HCM Control Delay, s	11.2			0			0					
HCM LOS	B											
Minor Lane/Major Mvmt	NBT	NBR	WBLn1	SBT	SBR							
Capacity (veh/h)	-	-	616	-	-							
HCM Lane V/C Ratio	-	-	0.06	-	-							
HCM Control Delay (s)	-	-	11.2	-	-							
HCM Lane LOS	-	-	B	-	-							
HCM 95th %tile Q(veh)	-	-	0.2	-	-							

HCM 6th TWSC  
2: I-84 EB On/Off Ramp & Bombing Range Rd

Data Center Due Dilligence  
2024 PM Peak Hour Build Conditions

**Intersection**

Int Delay, s/veh 3.9

Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations		↗	↘		↖	↗
Traffic Vol, veh/h	0	115	195	43	50	5
Future Vol, veh/h	0	115	195	43	50	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	400	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	125	212	47	54	5

**Major/Minor**

	Minor1	Major1	Major2
Conflicting Flow All	-	236	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	6.22	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	3.318	-
Pot Cap-1 Maneuver	0	803	-
Stage 1	0	-	-
Stage 2	0	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	803	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

**Approach**

	NW	NE	SW
HCM Control Delay, s	10.3	0	7.2
HCM LOS	B		

**Minor Lane/Major Mvmt**

	NET	NERNWLn1	SWL	SWT
Capacity (veh/h)	-	-	803	1306
HCM Lane V/C Ratio	-	-	0.156	0.042
HCM Control Delay (s)	-	-	10.3	7.9
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.6	0.1

HCM 6th TWSC  
4: Bombing Range Rd & Frontage Rd

Data Center Due Dilligence  
2024 PM Peak Hour Build Conditions

Intersection						
Int Delay, s/veh	0.5					
Movement	NBT	NBR	SBL	SBT	SWL	SWR
Lane Configurations	↔			↕	↕	
Traffic Vol, veh/h	150	5	5	83	5	5
Future Vol, veh/h	150	5	5	83	5	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	163	5	5	90	5	5
Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	168	0	266	166
Stage 1	-	-	-	-	166	-
Stage 2	-	-	-	-	100	-
Critical Hdwy	-	-	4.12	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.218	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1410	-	723	878
Stage 1	-	-	-	-	863	-
Stage 2	-	-	-	-	924	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1410	-	720	878
Mov Cap-2 Maneuver	-	-	-	-	720	-
Stage 1	-	-	-	-	863	-
Stage 2	-	-	-	-	920	-
Approach	NB	SB	SW			
HCM Control Delay, s	0	0.4	9.6			
HCM LOS						A
Minor Lane/Major Mvmt	NBT	NBR	SBL	SBTSWLn1		
Capacity (veh/h)	-	-	1410	-	791	
HCM Lane V/C Ratio	-	-	0.004	-	0.014	
HCM Control Delay (s)	-	-	7.6	0	9.6	
HCM Lane LOS	-	-	A	A	A	
HCM 95th %tile Q(veh)	-	-	0	-	0	

HCM 6th TWSC  
5: Bombing Range Rd & Wilson Lane

Data Center Due Dilligence  
2024 PM Peak Hour Build Conditions

Intersection						
Int Delay, s/veh	2.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	W			↑	↑	
Traffic Vol, veh/h	40	15	15	115	43	45
Future Vol, veh/h	40	15	15	115	43	45
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	43	16	16	125	47	49

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	229	72	96	0	-	0
Stage 1	72	-	-	-	-	-
Stage 2	157	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	759	990	1498	-	-	-
Stage 1	951	-	-	-	-	-
Stage 2	871	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	751	990	1498	-	-	-
Mov Cap-2 Maneuver	751	-	-	-	-	-
Stage 1	941	-	-	-	-	-
Stage 2	871	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.8	0.9	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1498	-	804	-	-
HCM Lane V/C Ratio	0.011	-	0.074	-	-
HCM Control Delay (s)	7.4	0	9.8	-	-
HCM Lane LOS	A	A	A	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-

HCM 6th TWSC  
6: Bombing Range Rd & Project Site Access

Data Center Due Dilligence  
2024 PM Peak Hour Build Conditions

Intersection						
Int Delay, s/veh	2.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	Y			4	4	
Traffic Vol, veh/h	55	0	0	75	35	23
Future Vol, veh/h	55	0	0	75	35	23
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	60	0	0	82	38	25

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	133	51	63	0	-	0
Stage 1	51	-	-	-	-	-
Stage 2	82	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	861	1017	1540	-	-	-
Stage 1	971	-	-	-	-	-
Stage 2	941	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	861	1017	1540	-	-	-
Mov Cap-2 Maneuver	861	-	-	-	-	-
Stage 1	971	-	-	-	-	-
Stage 2	941	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.5	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1540	-	861	-	-
HCM Lane V/C Ratio	-	-	0.069	-	-
HCM Control Delay (s)	0	-	9.5	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0.2	-	-



- GENERAL NOTES**
- 1. SEE ELECTRICAL SPECIFICATIONS # 18-0000 FOR GENERAL NOTES.
  - 2. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTALLATION INSTRUCTIONS.
  - 3. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE (NEC) AND ALL APPLICABLE LOCAL CODES.
  - 4. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 5. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 6. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 7. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 8. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 9. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.
  - 10. ALL LIGHTING FIXTURES SHALL BE INSTALLED IN ACCORDANCE WITH THE ILLINOIS ELECTRICAL CODE (IEC) AND ALL APPLICABLE LOCAL CODES.

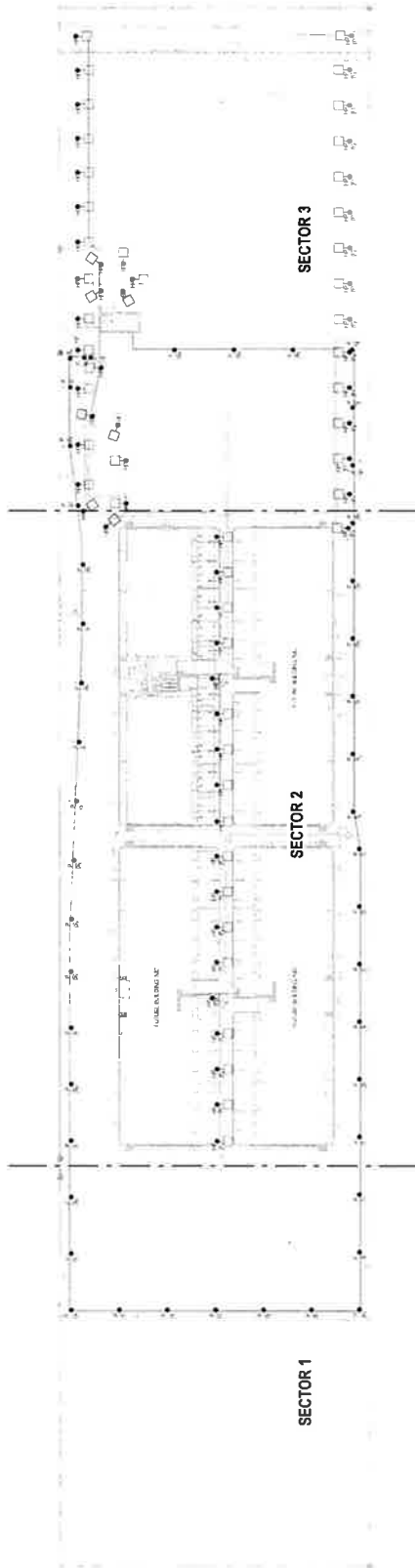
**SHEET NOTES**

**CONFIDENTIAL**

PROJECT NUMBER:  
BOMBING RANGE ROAD  
BOARDMAN, OR 97818

PERMIT SET

DATE: 08/14/2018



**SITE LIGHTING OVERALL PLAN**

H	EXTERIOR ARCHITECTURAL AREA LUMINAIRE - WALL MOUNT	UTRONA DS10 LED 300 700 40K T8M HVOLT PER7 W8A D085D MCGRAW HILL EDISON ICSOPER, TLM-F04-LED-E1-SL4-BZ-WMP	277	117 103 86.7	37' AFF	PROVIDE WITH FIXTURE MOUNTED PHOTOCELL	41
HE	EXTERIOR ARCHITECTURAL WALL-PAK	UTRONA DS10 LED 300 700 40K T8M HVOLT PER7 W8A D085D MCGRAW HILL EDISON ICSOPER, TLM-F04-LED-E1-SL4-BZ-WMP	277	27.9 27.9 41.1'	1' ABOVE FINISHED DOOR	PROVIDE EN BATTERY WHERE INDICATED PROVIDE WITH FIXTURE MOUNTED PHOTOCELL*	30
HP	EXTERIOR ARCHITECTURAL AREA LUMINAIRE - POLE MOUNT	UTRONA DS10 LED 300 700 40K T8M HVOLT PER7 SPA D085D MCGRAW HILL EDISON ICSOPER, TLM-F04-LED-E1-SL4-BZ-DMP	277	117 103 86.7'	30' AFF	PROVIDE WITH FIXTURE MOUNTED PHOTOCELL POLE MOUNT	34

PROJECT NUMBER: BOMBING RANGE ROAD BOARDMAN, OR 97818

DATE: 08/14/2018

PROJECT LOCATION: BOARDMAN, OR 97818

PROJECT OWNER: [REDACTED]

DESIGNER: [REDACTED]

SCALE: [REDACTED]

PROJECT NUMBER: BOMBING RANGE ROAD BOARDMAN, OR 97818

DATE: 08/14/2018

PROJECT LOCATION: BOARDMAN, OR 97818

PROJECT OWNER: [REDACTED]

DESIGNER: [REDACTED]

SCALE: [REDACTED]

PROJECT NUMBER: BOMBING RANGE ROAD BOARDMAN, OR 97818

DATE: 08/14/2018

PROJECT LOCATION: BOARDMAN, OR 97818

PROJECT OWNER: [REDACTED]

DESIGNER: [REDACTED]

SCALE: [REDACTED]



**From:** [Peacher, Kimberly N CIV USN NAVFAC NW SVD WA \(USA\)](#)  
**To:** [Jesse Walt](#)  
**Cc:** [Tamra Mabbott](#); [King, Seth J. \(POR\)](#)  
**Subject:** RE: AWS and NAS connect  
**Date:** Tuesday, March 15, 2022 2:07:34 PM  
**Attachments:** [image001.jpg](#)  
[image002.jpg](#)  
[image003.jpg](#)  
[image004.png](#)

---

Hello Jesse,

This is just what I needed. Based on this additional information there are no concerns from our end.

Appreciate the opportunity to review.

Thank you.

V/R,

Kimberly Peacher  
 Community Planning & Liaison Officer  
 Northwest Training Range Complex  
 (360) 930-4085

**From:** Jesse Walt <jessew@ygh.com>  
**Sent:** Tuesday, March 15, 2022 12:24 PM  
**To:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA) <kimberly.n.peacher.civ@us.navy.mil>  
**Cc:** Tamra Mabbott <tmabbott@co.morrow.or.us>; King, Seth J. (POR) <sking@perkinscoie.com>  
**Subject:** [URL Verdict: Neutral][Non-DoD Source] RE: AWS and NAS connect

Kimberly,

Attached is the cutsheet of the specified area lighting. You can see from the image that the fixture directional and intended to only illuminate in a downward direction from its mounting location.

Let me know if you need anything else.

Thank you

[Jesse Walt, AIA, LEED AP BD+C](#) | Shareholder | he/him  
[Yost Grube Hall Architecture](#)  
 707 SW Washington Street, Suite 1200 | Portland, OR 97205 | USA  
 t 503 221 0150 | d 503 715 3214 | m 503 449 2896 | w [ygh.com](http://ygh.com)



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**From:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA)  
<[kimberly.n.peacher.civ@us.navy.mil](mailto:kimberly.n.peacher.civ@us.navy.mil)>  
**Sent:** Tuesday, March 15, 2022 11:37 AM  
**To:** Jesse Walt <[jessew@ygh.com](mailto:jessew@ygh.com)>  
**Cc:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>; King, Seth J. (POR) <[sking@perkinscoie.com](mailto:sking@perkinscoie.com)>  
**Subject:** RE: AWS and NAS connect

Hello Jesse,

In talking to the Range folks, we just want reassurance that the fixtures intended for the poles and overhead lights are directional (i.e. shielded downward or northerly (on southern property line)). Can you please verify if that is the case?

Thank you.

V/R,

Kimberly Peacher  
Community Planning & Liaison Officer  
Northwest Training Range Complex  
(360) 930-4085

**From:** Jesse Walt <[jessew@ygh.com](mailto:jessew@ygh.com)>  
**Sent:** Monday, March 14, 2022 4:30 PM  
**To:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA) <[kimberly.n.peacher.civ@us.navy.mil](mailto:kimberly.n.peacher.civ@us.navy.mil)>;  
King, Seth J. (POR) <[sking@perkinscoie.com](mailto:sking@perkinscoie.com)>  
**Cc:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>  
**Subject:** [URL Verdict: Neutral][Non-DoD Source] RE: AWS and NAS connect

Hello Kimberly,

I've attached a composite of the site lighting plan and fixture schedule from our pending permit submission (stamp redacted). I suspect you may need additional information, such as photometrics, but do not have that readily available.

Please let me know what additional information, if any, is required.

If any adjustments need to be made to the site lighting, can you please also supply a copy of the standards we need to meet? I've been unable to locate them online.

Thank you

Jesse Walt, AIA, LEED AP BD+C | Shareholder | he/him

Yost Grube Hall Architecture  
 707 SW Washington Street, Suite 1200 | Portland, OR 97205 | USA  
 t 503 221 0150 | d 503 715 3214 | m 503 449 2896 | w [ygh.com](http://ygh.com)



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**From:** Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA)  
 <[kimberly.n.peacher.civ@us.navy.mil](mailto:kimberly.n.peacher.civ@us.navy.mil)>  
**Sent:** Monday, March 14, 2022 3:48 PM  
**To:** King, Seth J. (POR) <[sking@perkinscoie.com](mailto:sking@perkinscoie.com)>; Jesse Walt <[jessew@ygh.com](mailto:jessew@ygh.com)>  
**Cc:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>  
**Subject:** RE: AWS and NAS connect

Hello Seth,

Thank you for reaching out.

Jesse,

When ready, can you please forward the lighting study and or plans?

Thank you.

V/R,

Kimberly Peacher  
 Community Planning & Liaison Officer  
 Northwest Training Range Complex  
 (360) 930-4085

**From:** King, Seth J. (POR) <[sking@perkinscoie.com](mailto:sking@perkinscoie.com)>  
**Sent:** Monday, March 14, 2022 3:13 PM  
**To:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>; Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA) <[kimberly.n.peacher.civ@us.navy.mil](mailto:kimberly.n.peacher.civ@us.navy.mil)>  
**Cc:** Jesse Walt <[jessew@ygh.com](mailto:jessew@ygh.com)>  
**Subject:** [URL Verdict: Neutral][Non-DoD Source] RE: AWS and NAS connect

Thanks, Tamra. Kimberly: I'm following up on this item, which the Planning Commission continued from its 2/22 meeting to a later date. I have copied Jesse Walt, who is the design lead for the project and is familiar with the proposed lighting for the project. Please reach out to Jesse if you have questions or concerns about the project lighting. Thank you.

**Seth King | Perkins Coie LLP**

**PARTNER**  
1120 N.W. Couch Street Tenth Floor  
Portland, OR 97209-4128  
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E. [sking@perkinscoie.com](mailto:sking@perkinscoie.com)

**Read our Commitment to Racial Equality:** [www.perkinscoie.com/racialequality](http://www.perkinscoie.com/racialequality)

**From:** Tamra Mabbott <[tmabbott@co.morrow.or.us](mailto:tmabbott@co.morrow.or.us)>  
**Sent:** Tuesday, February 22, 2022 10:00 AM  
**To:** 'Peacher, Kimberly N CIV USN NAVFAC NW SVD WA (USA)' <[kimberly.n.peacher.civ@us.navy.mil](mailto:kimberly.n.peacher.civ@us.navy.mil)>; King, Seth J. (POR) <[sking@perkinscoie.com](mailto:sking@perkinscoie.com)>  
**Subject:** AWS and NAS connect

This is an e-introduction.

Kimberly Peacher is the Liaison Officer for NAS Whidbey Island.  
Seth King is Attorney for AWS.

Kim – Mr. King would like to talk with you about the glare and lighting concern at the AWS data Center which is on the docket for Planning Commission tonight.

Cordially, Tamra

Tamra Mabbott  
Planning Director  
Morrow County  
PO Box 40  
205 Third Street NE  
Irrigon, OR 97844  
(541) 922-4624 X5505



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**PRELIMINARY FINDINGS OF FACT  
LAND PARTITION/REPLAT REQUEST  
LP-N-508-22  
R-N-076-22**

**REQUEST:** To partition an approximate 64,812-acre property into two parcels.

---

**APPLICANT:** Rowan Green Data LLC  
600 A Avenue  
Lake Oswego, OR 97034

**OWNER:** Threemile Canyon Farms  
75906 Threemile Road  
Boardman, OR 97818

**PROPERTY DESCRIPTION:** Tax Lot 100 of Assessor's Map 2N 23E; tax lot 100 of 2N 24E; tax lot 100 of 3N 23E; tax lot 100 of 3N 24E; tax lot 110 of 4N 23E; and tax lot 121 of 4N 24E

**PROPERTY DESCRIPTION:** Approximately 10 miles Southwest of Boardman on Tower Road

**I FINDINGS OF FACT:**

The subject parcel is zoned Exclusive Farm Use (EFU) with a minimum lot size of 160 acres. The subject property is presently bare. The applicant is proposing to divide the subject property into two parcels, one proposed at over 64,500 acres and the other at approximately 275 acres.

**APPROVAL CRITERIA:**

Morrow County Zoning Ordinance Article 3 Exclusive Farm Use Zone (EFU) and Morrow County Subdivision Ordinance Article 5 Land Partitions will apply. Criteria are listed below in **bold type**, followed by a response in standard type.

**MORROW COUNTY ZONING ORDINANCE ARTICLE 3 USE ZONES SECTION**

**3.010(L). LAND DIVISIONS.**

1. **Minimum Parcel Size. The minimum size for creation of a new parcel shall be 160 acres.**  
The new parcels are proposed to be more than 160 acres. This criterion is met.
2. **A division of land to accommodate a use permitted by Section C, except a residential use, smaller than the minimum parcel size provided in Subsection 1 may be approved if the parcel for the nonfarm use is not larger than the minimum size necessary for the use.**  
This criterion is not applicable as the proposal is larger than the minimum parcel size.
3. **A division of land to create up to two new parcels smaller than the minimum size established under Subsection 1, each to contain a dwelling not provided in conjunction with farm use, may be permitted if:**
  - a. **The nonfarm dwellings have been approved under Subsection H;**

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Findings of Fact

- b. **The parcels for the nonfarm dwellings are divided from a lot or parcel that was lawfully created prior to July 1, 2001;**
- c. **The parcels for the nonfarm dwellings are divided from a lot or parcel that complies with the minimum size in Subsection 1; and**
- d. **The remainder of the original lot or parcel that does not contain the nonfarm dwellings complies with the minimum size established under Subsection 1.**

This criterion is not applicable as the division of land is not for the purpose of creating nonfarm dwelling parcels.

4. **A division of land to divide a lot or parcel into two parcels, each to contain one dwelling not provided in conjunction with farm use, may be permitted if:**
- a. **The nonfarm dwellings have been approved under Subsection H;**
  - b. **The parcels for the nonfarm dwellings are divided from a lot or parcel that was lawfully created prior to July 1, 2001;**
  - c. **The parcels for the nonfarm dwellings are divided from a lot or parcel that is equal to or smaller than the minimum size in Subsection A but equal to or larger than 40 acres;**
  - d. **The parcels for the nonfarm dwellings are:**
    - (1) **Not capable of producing more than at least 20 cubic feet per acre per year of wood fiber; and**
    - (2) **Either composed of at least 90 percent Class VII and VIII soils, or composed of at least 90 percent Class VI through VIII soils and are not capable of producing adequate herbaceous forage for grazing livestock. The Land Conservation and Development Commission, in cooperation with the State Department of Agriculture and other interested persons, may establish by rule objective criteria for identifying units of land that are not capable of producing adequate herbaceous forage for grazing livestock. In developing the criteria, the commission shall use the latest information from the United States Natural Resources Conservation Service and consider costs required to utilize grazing lands that differ in acreage and productivity level; and**
  - e. **The parcels for the nonfarm dwellings do not have established water rights for irrigation.**

This criterion is not applicable as the request is not for dwellings not in conjunction with farm use.

5. **This Section does not apply to the creation or sale of cemetery lots, if a cemetery is within the boundaries designated for a farm use zone at the time the zone is established.**

This criterion is not applicable as this approval is not for the creation or sale of cemetery lots.

6. **This Section does not apply to divisions of land resulting from lien foreclosures or divisions of land resulting from foreclosure of recorded contracts for the sale of real property.**

This criterion is not applicable as the division of land is not resulting from a lien foreclosure or foreclosure of a recorded contract.

7. **This Section does not allow a division or a property line adjustment of a lot or parcel that separates a use described in B.29, C.3, or C.7 from the lot or parcel on which the primary residential use exists.**

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This criterion is not applicable as the division does not contain the uses mentioned, identified as farm worker dwellings, temporary hardship dwellings, or home occupations.

8. **This Section does not allow a division or a property line adjustment of a lot or parcel that separates a processing facility from the farm operation specified in Section B.17.**

This criterion is not applicable as the division does not separate a processing facility from the farm operation.

9. **A division of land may be permitted to create a parcel with an existing dwelling to be used:**

a. **As a residential home as described in ORS 197.660 (2) only if the dwelling has been approved under Section H; and**

b. **For historic property that meets the requirements of Section B.33.**

This criterion is not applicable as the property does not have an existing dwelling.

10. **Notwithstanding the minimum lot or parcel size described in Subsection 1,**

a. **A division of land may be approved provided:**

(1) **The land division is for the purpose of allowing a provider of public parks or open space, or a not-for-profit land conservation organization, to purchase at least one of the resulting parcels; and**

(2) **A parcel created by the land division that contains a dwelling is large enough to support continued residential use of the parcel.**

b. **A parcel created pursuant to this Subsection that does not contain a dwelling:**

(1) **Is not eligible for siting a dwelling, except as may be authorized under ORS 195.120;**

(2) **May not be considered in approving or denying an application for siting any other dwelling;**

(3) **May not be considered in approving a redesignation or rezoning of forestlands except for a redesignation or rezoning to allow a public park, open space or other natural resource use; and**

(4) **May not be smaller than 25 acres unless the purpose of the land division is to facilitate the creation of a wildlife or pedestrian corridor or the implementation of a wildlife habitat protection plan or to allow a transaction in which at least one party is a public park or open space provider, or a not-for-profit land conservation organization, that has cumulative ownership of at least 2,000 acres of open space or park property.**

This criterion is not applicable as this is not an application for a public park, open space, or a land conservation organization.

11. **A division of land smaller than the minimum lot or parcel size in Subsection 1 may be approved provided:**

a. **The division is for the purpose of establishing a church, including cemeteries in conjunction with the church;**

b. **The church has been approved under Subsection B.26;**

c. **The newly created lot or parcel is not larger than five acres; and**

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Findings of Fact

- d. **The remaining lot or parcel, not including the church, meets the minimum lot or parcel size described in Subsection 1 either by itself or after it is consolidated with another lot or parcel.**

This criterion is not applicable as the division is not for the purpose of establishing a church or cemetery.

12. **Notwithstanding the minimum lot or parcel size described Subsection 1, a division for the nonfarm uses set out in Subsection B.12 if the parcel for the nonfarm use is not larger than the minimum size necessary for the use.**

This criterion is not applicable as the division is not for a fire service facility as indicated in Subsection B.12.

13. **The governing body of a county may not approve a division of land for nonfarm use under Subsection 2, 3, 4, 9, 10, 11, or 12 unless any additional tax imposed for the change in use has been paid.**

This criterion is not applicable as the proposal is not for a nonfarm dwelling.

14. **Parcels used or to be used for training or stabling facilities may not be considered appropriate to maintain the existing commercial agricultural enterprise in an area where other types of agriculture occur.**

This criterion is not applicable as the parcel is not used for training or stabling facilities.

15. **A division of a lawfully established unit of land may occur along an urban growth boundary where the parcel remaining outside the urban growth boundary is zoned for agricultural uses and is smaller than the minimum parcel size, provided that:**

- a. **If the parcel contains a dwelling, the parcel must be large enough to support the continued residential use.**
- b. **If the parcel does not contain a dwelling, it:**
- (1) **Is not eligible for siting a dwelling, except as may be authorized in ORS 195.120;**
  - (2) **May not be considered in approving or denying an application for any other dwelling; and**
  - (3) **May not be considered in approving a redesignation or rezoning of agricultural lands, except to allow a public park, open space, or other natural resource use.**

This criterion is not applicable as the land is not along an urban growth boundary.

**MORROW COUNTY SUBDIVISION ORDINANCE ARTICLE 5 LAND PARTITIONS  
SECTION 5.030. REQUIREMENTS FOR APPROVAL. No application for partitioning shall be approved unless the following requirements are met:**

1. **Proposal is in compliance with the County and affected City Comprehensive Plan and applicable Zoning.**

The proposed land partition meets the requirements of the Zoning Ordinance and Comprehensive Plan as outlined in these Findings. The applicant will need to submit both a preliminary and final partition plat to be in compliance with this Article and ORS Chapter 92. This is recommended and listed as a Condition of Approval.

2. **Each parcel is suited for the use intended or offered; including, but not limited to, sewage disposal approval and guaranteed access.**

The applicant has requested one new parcel which is proposed to be approximately 275 acres in area. Both proposed parcels have direct frontage on

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Findings of Fact



Tower Road and are large enough to accommodate onsite sewage systems. This criterion is met.

3. **All required public service and facilities are available and adequate or are proposed to be provided by the partitioner.**  
Electricity and telephone are available to the existing parcel from nearby Tower Road. The applicant will have to work with providers to obtain any necessary services to the proposed parcels.
4. **Proposal will not have any identifiable adverse impacts on adjoining or area land uses, public services and facilities, and natural resource carrying capacities.**  
The surrounding uses are farm land and industrial land. The lands to the east and south are the location of the Carty Generating Station and the former Coal Plant. This land partition would not have an effect on adjoining land uses. This criterion is met.
5. **An approved water rights diversion plan as applicable.**  
The preliminary Findings of Fact were referred to the County Water master to determine water rights and for any necessary approvals.
6. **Flag lots will not be permitted when the results would be to increase the number of properties requiring direct and individual access from a State Highway or other arterial. Flag lots may be permitted to achieve planning objectives under the following conditions:**
  - a. **When flag lot driveways are separated by at least twice the minimum frontage distance.**
  - b. **The driveway must meet driveway standards described in Article 8, Section 8.020.V.**
  - c. **The lot meets the minimum lot area of the zoning district, without including the driveway.**
  - d. **Only one flag lot shall be permitted per private right-of-way or access easement.**

This provision does not apply as no flag lots are proposed.
7. **The depth of any lot will not be restricted as long as a buildable parcel is proposed.**  
The application meets this criterion as a buildable parcel is proposed.
8. **No plat of a subdivision or partition located within the boundaries of an irrigation district, drainage district, water control district, water improvement district or district improvement company will be approved unless the County has received and accepted a certification from the district or company that the subdivision or partition is either entirely excluded from the district or company or is included within the district or company for purposes of receiving services and subjecting the subdivision or partition to the fees and other charges of the district or company.**  
This criterion is not applicable as the property is not within an irrigation district, drainage district, or district improvement company boundary.
9. **The Commission will deny an application for partitioning when it can be shown by the Commission that the partitioning is part of a plan or scheme to create more than three (3) parcels without going through subdivision, or is part of a development pattern creating more than three (3) parcels without subdividing.**  
This provision does not apply to this application.

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Findings of Fact

10. In addition to the requirements set forth above, the following factors may be considered for approval or disapproval of an application for land partitioning is a geographical or other factor identified by other, appropriate professionals or Plans such as the requirements of the Comprehensive Plan, FEMA requirements, Byways rules, etc., requires it:
- a. Placement and availability of utilities.
  - b. Safety from fire, flood and other natural hazards.
  - c. The same improvements may be required for a partitioning as required of a subdivision, if required it will be installed by the applicant.
  - d. Possible effects on natural, scenic and historical resources.
  - e. Need for onsite or offsite improvements.
  - f. Need for additional setback, screening, landscaping and other requirements relative to the protection of adjoining and area land uses. If the proposed partition is located within an Urban Growth Boundary, the affected city must be given notice according to the respective Joint Management agreement.
  - g. In the approval of a land partition, the need for street and other improvements will be considered and may be required as a Condition of Approval at a different standard than for a subdivision.

Planning staff would not recommend any additional requirements based on the factors above.

**SECTION 5.075. REPLATTING.** A reconfiguration of a recorded subdivision or partition plat or a change in the number of lots in the subdivision or partition may be approved by the Planning Commission or as defined in ORS 92.180. Replats will act to vacate the platted lots or parcels and easements within the replat area with the following conditions:

1. **A replat will apply only to a recorded plat.**  
A replat is required as the applicant is proposing to partition Parcel 1 of Partition Plat 2012-3.
2. **Notice shall be provided when the replat is replatting all of an undeveloped subdivision as defined in ORS 92.225.**  
This provision does not apply as this is not a replat of an undeveloped subdivision. Notice was provided under the requirements of a Land Partition.
3. **Notice shall be provided to the owners of property adjacent to the exterior boundaries of the tentative subdivision replat.**  
Adjoining property owner notice was provided on March 8, 2022.
4. **When a utility easement is proposed to be realigned, reduced in width or omitted by a replat, all affected utility companies or public agencies shall be notified, consistent with a governing body's notice to owners of property contiguous to the proposed plat. Any utility company that desires to maintain an easement subject to vacation under this section must notify the Planning Department in writing within 14 days of the mailing or other service of the notice.**  
No changes to utility easements are proposed. This replat is at the request of the applicant.
5. **A replat will not serve to vacate any public street or road.**

LP-N-508-22

R-N-076-22

Findings of Fact

No streets or roads are proposed to be vacated.

**6. A replat will comply with all subdivision provisions of this Article and all applicable Ordinances.**

See Land Partition requirements above.

- III LEGAL NOTICE PUBLISHED:** March 8, 2022  
East Oregonian
- March 9, 2022  
Heppner Gazette Times
- IV AGENCIES NOTIFIED:** Josh Lebombard, Department of Land Conservation and Development; Linda Hayes-Gorman, Department of Environmental Quality; Greg Silbernagel, Water master; Michael Hughes, Boardman Rural Fire Protection District; Kimberly Peacher, US Navy; Mike Gorman, Morrow County Assessor; Eric Imes, Morrow County Public Works Director; Matt Kenny, Morrow County Surveyor.
- V PROPERTY OWNERS NOTIFIED:** March 8, 2022
- VI HEARING DATE:** March 29, 2022  
North Morrow Government Center  
Irrigon, Oregon
- VII PLANNING COMMISSION ACTION:** Staff recommend approval subject to the following **Precedent Conditions of Approval**. These conditions must be completed before the Planning Director may sign the final partition plat or before the land may be deeded to a third party.
1. Submit both a preliminary and final partition plat to be in compliance with Article 5 of the Morrow County Subdivision Ordinance and ORS Chapter 92.

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Jeff Wenholz, Chair

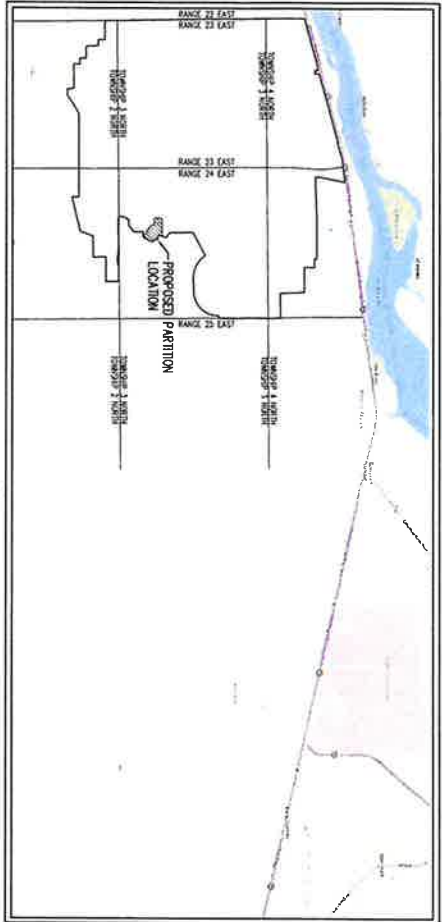
Date

**ATTACHMENTS:**  
Vicinity Map  
Preliminary Plan

**LP-N-508-22**

**R-N-076-22**

**Findings of Fact**



**VISINITY MAP**

NOT TO SCALE



**PERCHERON PROJECT**  
**PARTITION APPLICATION**  
 LOCATED IN PORTIONS OF TOWNSHIPS 2 & 3 & 4 NORTH, RANGES 23 & 24  
 WILLAMETTE MERIDIAN, MORROW COUNTY, OREGON

**SHEET INDEX:**

SHEET-01	COVER SHEET
SHEET-02	OVERALL PARTITION PLAN MAP
SHEET-03	DETAILED PARTITION PLAN MAP
SHEET-04	DETAILED PARTITION PLAN MAP- AERIAL

**APPLICANT**  
 ROMAN GREEN DATA, LLC

**PLANNING / ENGINEERING / SURVEYING FIRM**  
 AKS ENGINEERING & FORESTRY, LLC.  
 CONTACT: CODY STREET  
 12963 SW HERMAN ROAD, SUITE 100  
 TUALUMIN, OR 97062  
 PHONE: (503) 563-6151

**ZONE:** RRI (PROPOSED PARCEL 1)  
 RRI, SA AND RU (PROPOSED PARCEL 2)

**WATER DISTRICT:** WELL (NONE ON PROPOSED PARCEL 1)

**SEWER DISTRICT:** SEPTIC (NONE ON PROPOSED PARCEL 1)

**PROPERTY ADDRESS:** N/A

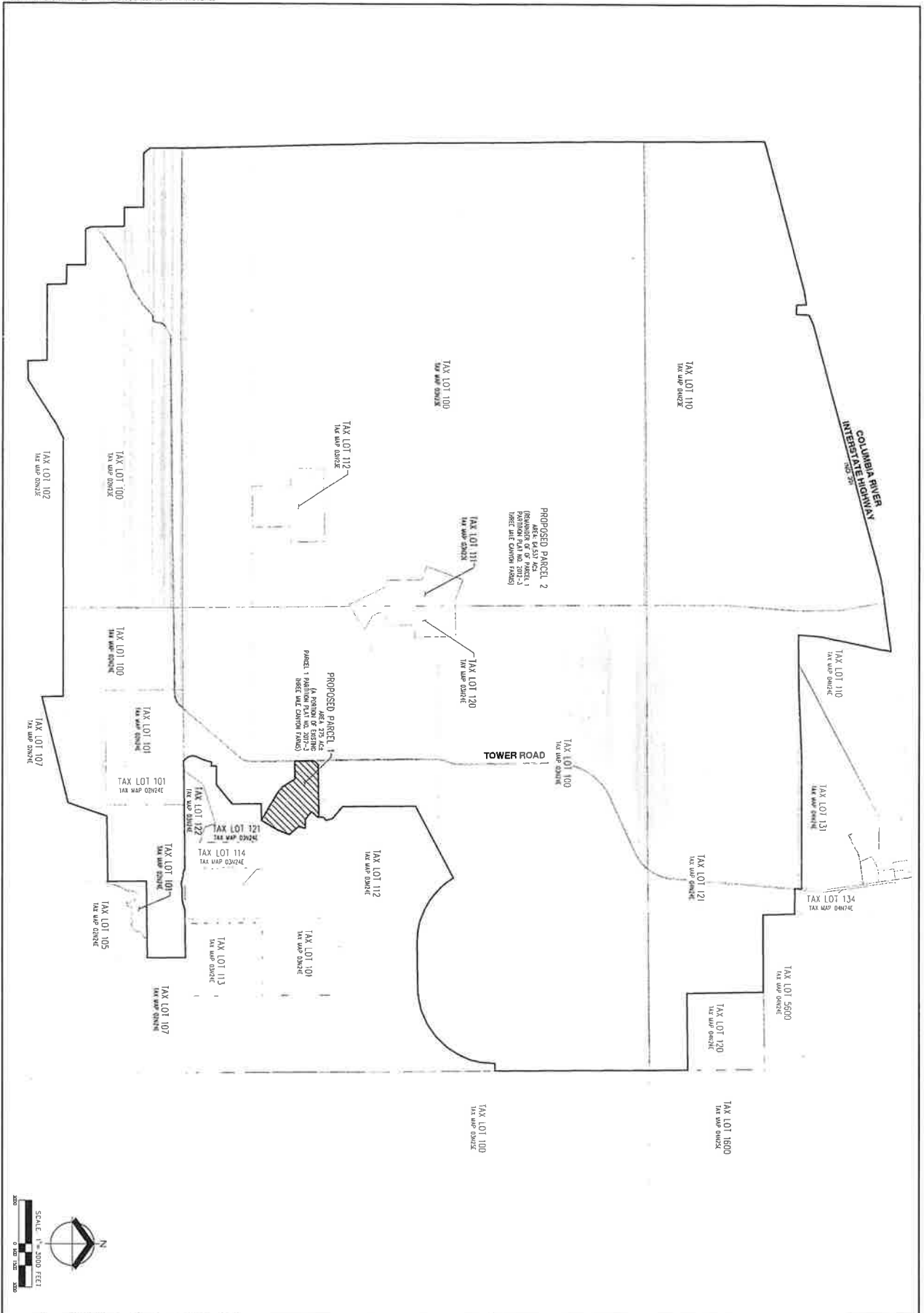
**PROPERTY DESCRIPTION:** PARCEL 1 OF PARTITION PLAT NO. 2012-3 MORROW COUNTY SURVEY RECORDS

**PROJECT SITE AREA:** PARCEL PARCELS AREA: 64.912 AC±  
 PARTITION PARCELS AREA: 64.537 AC±  
 REMAINDER OF PARTITION PARCEL AREA: 64.537 AC±

**PROJECT PURPOSE:** PARTITION APPLICATION



<p><b>PERCHERON PROJECT</b></p> <p><b>COVER SHEET</b></p>	<p><b>AKS ENGINEERING &amp; FORESTRY, LLC</b>                  12963 SW HERMAN RD. SUITE 100                  TUALUMIN, OR 97062                  503.563.6151                  WWW.AKS-INC.COM</p> <p>ENGINEERING - SURVEYING - NATURAL RESOURCES                  FORESTRY - PLANNING - LANDSCAPE ARCHITECTURE</p>	<p>PROJECT NO. 8858-04                  SHEET NO. 01</p>
---	--	--



<p><b>REVIEW COPY</b></p> <p>DATE: 11/17/2020</p> <p>PROJECT: PERCHERON PARTITIONING LAYOUT</p> <p>CLIENT: AKS ENGINEERING &amp; FORESTRY, LLC</p> <p>SCALE: 1" = 2000 FEET</p> <p>SHEET: 02</p>	<p><b>JOB NUMBER:</b> 8858-04</p> <p><b>DATE:</b> 11/17/2020</p> <p><b>SCALE:</b> 1" = 2000 FEET</p> <p><b>SHEET:</b> 02</p>
--	--

**OVERALL PARTITION PLAN MAP**

**PERCHERON PROJECT**

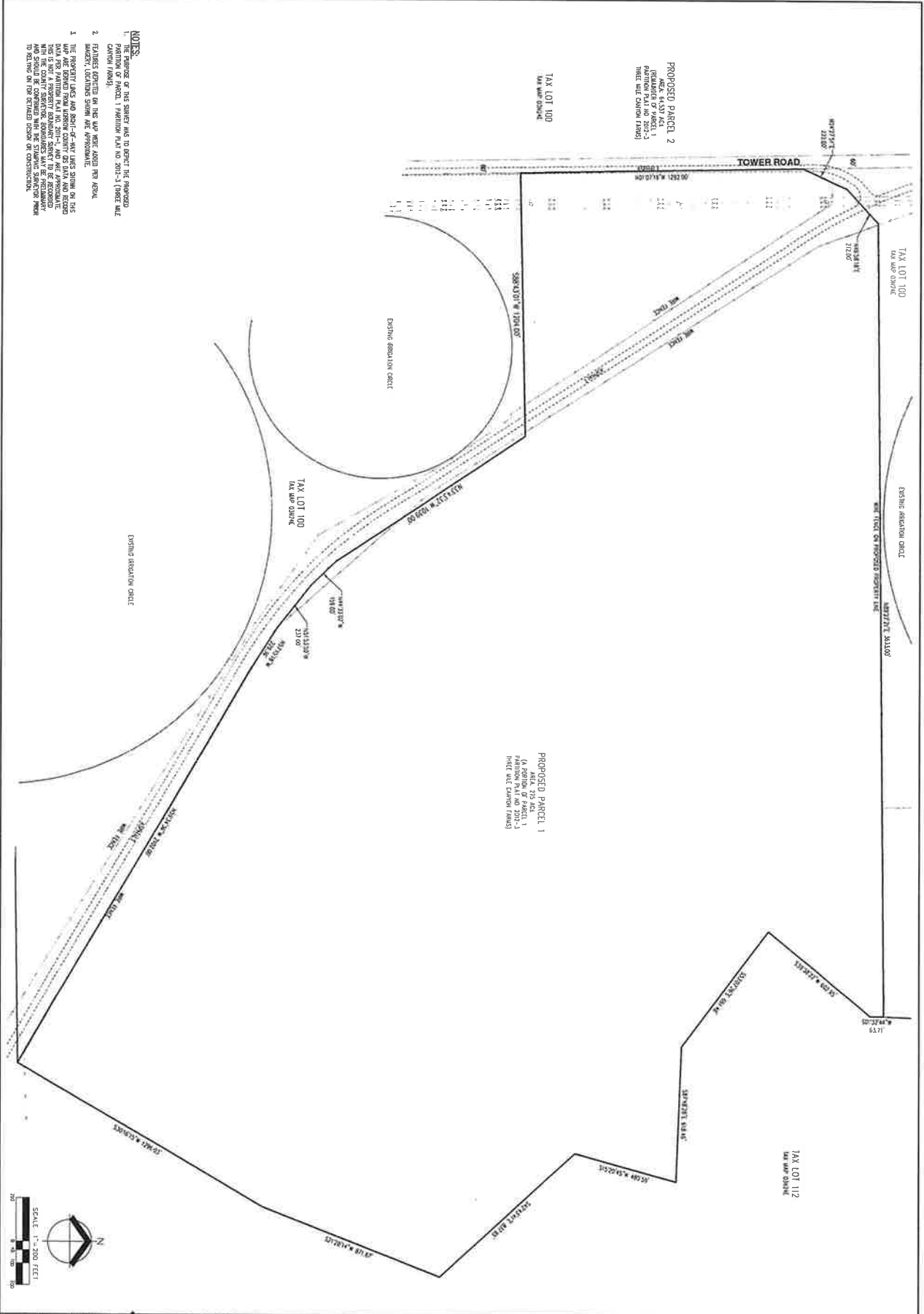
OREGON WASHINGTON COUNTY

AKS ENGINEERING & FORESTRY, LLC  
 12965 SW HERMAN RD, STE 100  
 TUALOIN, OR 97142  
 503.563.1511  
 WWW.AKS-INC.COM

**AKS**

ENGINEERING • SURVEYING • NATURAL RESOURCES  
 FORESTRY • PLANNING • LANDSCAPE ARCHITECTURE

AKS DRAWING FILE 2024-04 PERCHON PARTITION.DWG | LAYOUT | DETAILED PARTITION LAYOUT



- NOTES:**
1. THE PURPOSE OF THIS SURVEY WAS TO ADJUST THE PROPOSED PARTITION OF PARCELS 1 & 2 PERMITTED PLAN NO. 2014-31 THREE A.C.E. PARTITION PLAN.
  2. EXISTING EASEMENTS ON THE MAP WERE ADJUSTED PER ABEAL RECORDS, 6/21/2014, AS SHOWN ON THE MAP.
  3. THE NEW EASEMENT CIRCLE ON THE MAP WAS DETERMINED BY THE DATA FROM THE SURVEY, AND IS NOT TO BE CONSIDERED AS A REQUIREMENT WITH THE COUNTY'S SURVEYOR. BOUNDARIES MAY BE RECONCILED TO EASERS BY THE APPLICANT BEFORE OR AFTER CONVEYANCE.

**PROPOSED PARCEL 1**  
 AREA: 4.75 AC  
 (A PORTION OF PARCELS 1, 2 & 3 FROM THE PERCHON PARTITION PLAN)

**PROPOSED PARCEL 2**  
 AREA: 4.53 AC  
 (A PORTION OF PARCELS 1, 2 & 3 FROM THE PERCHON PARTITION PLAN)

<p style="font-size: 2em; font-weight: bold; transform: rotate(-45deg);">REVIEW COPY</p> <p>DATE: 1/17/2024</p> <p>BY: [Signature]</p> <p>FOR: [Signature]</p>	<p>ISSUED TO: [Name]</p> <p>DATE: [Date]</p> <p>BY: [Name]</p>
	<p>PROJECT: [Project Name]</p> <p>CLIENT: [Client Name]</p>
	<p>PROJECT NO: 2024-04</p> <p>DATE: 1/17/2024</p>

AKS ENGINEERING & FORESTRY, LLC  
 12965 SW HERMAN RD., STE 100  
 TULLAHAMA, OR 97262  
 503.545.4242  
 WWW.AKS-ENG.COM

**DETAILED PARTITION**  
**PLAN MAP**

**PERCHON**  
**PROJECT**

**AKS**  
 ENGINEERING & FORESTRY, LLC  
 12965 SW HERMAN RD., STE 100  
 TULLAHAMA, OR 97262  
 503.545.4242  
 WWW.AKS-ENG.COM

**OREGON**  
 WASHINGTON COUNTY

ENGINEERING - SURVEYING - NATURAL RESOURCES  
 FORESTRY - PLANNING - LANDSCAPE ARCHITECTURE

AKS DRAWING FILE: 8858-04 PERCHERON PARTITION MAP - AERIAL



- NOTES:**
1. THE BOUNDARIES OF THIS SURVEY WERE TO CORRECT THE PREVIOUS PARTITION MAP AND TO CORRECT THE PREVIOUS PARTITION MAP TO THE CURRENT PARTITION MAP.
  2. FEATURES SHOWN ON THIS MAP ARE NOT TO BE CONSIDERED AS A GUARANTEE OF ACCURACY. THE USER SHOULD BE AWARE THAT THE SURVEY WAS CONDUCTED IN ACCORDANCE WITH THE CURRENT SURVEYING PRACTICES AND STANDARDS.
  3. THE PROPERTY LINES AND BOUNDARIES SHOWN ON THIS MAP ARE BASED ON THE SURVEY DATA AND FIELD MEASUREMENTS. THIS IS NOT A REPRESENTATIVE SURVEY TO BE CONSIDERED AS A GUARANTEE OF ACCURACY. THE USER SHOULD BE AWARE THAT THE SURVEY WAS CONDUCTED IN ACCORDANCE WITH THE CURRENT SURVEYING PRACTICES AND STANDARDS.



<p><b>REVISIONS</b></p> <table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> </tr> <tr> <td>1</td> <td>12/12/22</td> <td>ISSUED FOR PERMIT</td> </tr> </table>	NO.	DATE	DESCRIPTION	1	12/12/22	ISSUED FOR PERMIT	<p><b>PROJECT INFORMATION</b></p> <p>PROJECT NO: 8858-04</p> <p>DATE: 12/12/2022</p> <p>SCALE: 1" = 200 FEET</p>
NO.	DATE	DESCRIPTION					
1	12/12/22	ISSUED FOR PERMIT					

**DETAILED PARTITION  
PLAN MAP - AERIAL**

**PERCHERON  
PROJECT**

OREGON  
WATSON COUNTY

AKS ENGINEERING & FORESTRY, LLC  
 12005 SW HERMAN ST. STE. 102  
 TULASTON, OR 97142  
 503.548.6161  
 WWW.AKS-ENG.COM

**AKS**

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 FORESTRY • PLANNING • LANDSCAPE ARCHITECTURE

04

JOB NUMBER  
8858-04  
SHEET

**FINAL FINDINGS OF FACT  
LAND PARTITION REQUEST  
LP-N-509-22**

**REQUEST:** To partition an approximate 9.70-acre property into two parcels.

---

**APPLICANT** Barbara Hug  
70907 Kunze Lane  
Boardman, Oregon 97818

**OWNERS** Barbara Hug  
70907 Kunze Lane  
Boardman, Oregon 97818

**PROPERTY DESCRIPTION:** Tax Lot 1000 of Assessor's Map 4N 25E 15

**PROPERTY LOCATION:** East of Boardman City Limits on Kunze Lane between Olson Road and Miller Road.

**FINDINGS OF FACT:**

**I. BACKGROUND INFORMATION:**

The subject parcel is zoned Farm Residential and is located outside the Boardman Urban Growth Boundary (UGB). Subject parcel has one existing dwelling and several outbuildings. The applicant is proposing to create two parcels, Parcel 1 is proposed at approximately 2-acres, and proposed parcel two and three at approximately 7.71-acres in size. Proposed used for the new parcels is residential.

**II APPROVAL CRITERIA:**

Morrow County Subdivision Ordinance Article 5 Land Partitions will apply. Criteria are listed below in **bold type**, followed by a response in standard type.

**MORROW COUNTY SUBDIVISION ORDINANCE ARTICLE 5 LAND PARTITIONS SECTION 5.030. REQUIREMENTS FOR APPROVAL. No application for partitioning shall be approved unless the following requirements are met:**

**1. Proposal is in compliance with the County and affected City Comprehensive Plan and applicable Zoning.**

The proposed land partition meets the requirements of the Zoning Ordinance and Comprehensive Plan relative to minimum parcel size. The applicant will need to submit both a preliminary and final partition plat to



be in compliance with this Article and ORS Chapter 92. This is recommended and listed as a Condition of Approval.

2. **Each parcel is suited for the use intended or offered; including, but not limited to, sewage disposal approval and guaranteed access.** The applicant has proposed two parcels in the Rural Residential Zone; each proposed parcel is of an adequate size as to satisfy the minimum 2-acre size for this zone. Applicant has identified the future use for the proposed parcels as residential. Proposed Parcel 1 will maintain the existing dwelling.

Proposed Parcel 1 will continue to use the existing county road access point on Kunze Lane. Proposed Parcel 2 has approximately 416' of frontage along Kunze Lane, this frontage is suitable for future access. Morrow County Public Works was provided notice and a copy of these findings. The Applicant will need to receive access approval through the Morrow County Public Works Department prior to the Planning Director signing the Final Plat This is recommended and listed as a Condition of Approval. The Applicant will need to show all easements, existing and proposed on the Preliminary and Final Plat prior to the Planning Director's signature. This is recommended and listed as a Condition of Approval.

To ensure buildability of Proposed Parcel 2, the Applicant will need to obtain site suitability from Umatilla County Health (UCo. Health). This is recommended and listed as a Condition of Approval.

3. **All required public service and facilities are available and adequate or are proposed to be provided by the partitioner.** Electricity and telephone services are available in the area, Applicant will need to work with local providers for future service. No other public services or facilities are available.
4. **Proposal will not have any identifiable adverse impacts on adjoining or area land uses, public services and facilities, and natural resource carrying capacities.** The current parcel is zoned residential and surrounding uses are residential in nature, this action will not prevent the proposed or surrounding parcels to continue to be used as such. The proposal will not have any identifiable adverse impacts on public services or natural resources of the area.
5. **An approved water rights diversion plan as applicable.** Preliminary Findings of Fact were referred to the County Watermaster and it is recommended that the applicant comply with any recommendations of the County Watermaster based on his review.
6. **Flag lots will not be permitted when the results would be to increase the number of properties requiring direct and individual access from a State Highway or other arterial. Flag lots may be permitted to achieve planning objectives under the following conditions:**

- a. **When flag lot driveways are separated by at least twice the minimum frontage distance.**
- b. **The driveway must meet driveway standards described in Article 8, Section 8.020.V.**
- c. **The lot meets the minimum lot area of the zoning district, without including the driveway.**
- d. **Only one flag lot shall be permitted per private right-of-way or access easement.**

This provision does not apply as no flag lots are proposed.

7. **The depth of any lot will not be restricted as long as a buildable parcel is proposed.**

The application meets this criterion as buildable parcels are being proposed and will meet the minimum acreage for the FR zone.

8. **No plat of a subdivision or partition located within the boundaries of an irrigation district, drainage district, water control district, water improvement district or district improvement company will be approved unless the County has received and accepted a certification from the district or company that the subdivision or partition is either entirely excluded from the district or company or is included within the district or company for purposes of receiving services and subjecting the subdivision or partition to the fees and other charges of the district or company.**

The applicant should be aware that this property is located in an area designated for water quality concerns by the Oregon Department of Environmental Quality. The subject property is in the Lower Umatilla Basin Groundwater Management Area (LUBGWMA). The Department of Environmental Quality designated the LUBGWMA in 1990 due to elevated nitrate concentrations in groundwater. It is recommended that wells used for drinking water be tested at least annually to determine nitrate concentrations. More information about the LUBGWMA can be found at <http://www.oregon.gov/deq/wq/programs/Pages/GWP-Management-Areas.aspx>

This property is within the West Extension Irrigation District (WEID) and were provided a copy of these findings.

9. **The Commission will deny an application for partitioning when it can be shown by the Commission that the partitioning is part of a plan or scheme to create more than three (3) parcels without going through subdivision, or is part of a development pattern creating more than three (3) parcels without subdividing.**

This provision does not apply to this application as no more than 3 parcels are currently being proposed.

10. **In addition to the requirements set forth above, the following factors may be considered for approval or disapproval of an application for land**

**partitioning is a geographical or other factor identified by other, appropriate professionals or plans such as the requirements of the Comprehensive Plan, FEMA requirements, Byways rules, etc., requires it:**

- a. Placement and availability of utilities.**
- b. Safety from fire, flood and other natural hazards.**
- c. The same improvements may be required for a partitioning as required of a subdivision, if required it will be installed by the applicant.**
- d. Possible effects on natural, scenic and historical resources.**
- e. Need for onsite or offsite improvements.**
- f. Need for additional setback, screening, landscaping and other requirements relative to the protection of adjoining and area land uses. If the proposed partition is located within an Urban Growth Boundary, the affected city must be given notice according to the respective Joint Management agreement.**
- g. In the approval of a land partition, the need for street and other improvements will be considered and may be required as a Condition of Approval at a different standard than for a subdivision. Planning staff would not recommend any additional requirements based on the factors above.**

**III LEGAL NOTICE PUBLISHED:** March 08, 2022  
East Oregonian

March 09, 2022  
Heppner Gazette-Times

**IV AGENCIES NOTIFIED:** Greg Silbernagel, Watermaster; Beverly Bridgewater, WEID; Mike Hughes, Boardman Rural Fire Protection District; Justin Nelson, Morrow County Counsel; Mike Gorman, Morrow County Assessor; Eric Imes, Morrow County Public Works Director; Matt Kenny, Morrow County Surveyor

**V PROPERTY OWNERS NOTIFIED:** March 08, 2022

**VI HEARING DATE:** March 29, 2022

Bartholomew Building  
110 North Court Street  
Heppner, Oregon 97836

**VII PLANNING COMMISSION ACTION:** Staff recommend approval subject to the following **Precedent Conditions of Approval**. These conditions must be completed before the Planning Director may sign the final partition plat or before the land may be deeded to a third party.

1. Submit both a preliminary and final partition plat to be in compliance with Article 5 of the Morrow County Subdivision Ordinance and ORS Chapter 92.
2. Obtain site-suitability from Umatilla County Health for Proposed Parcel 2.
3. Obtain approval and any necessary access permits from Morrow County Public Works.
4. Show any existing and proposed easements on the Final Partition Plat.

---

Jeff Wenholz, Chair

Date

**ATTACHMENTS:**

Vicinity Map  
Preliminary Plan

# Vicinity Map



City of Boardman



**LP-N-509-22**  
 Barbara Hug  
 4N 25E 15 TL1000

### Legend

Tax Lot

Subject Property

Urban Growth Boundary



Date Saved: 3/31/2022 7:58 AM

Cartography By: Stephen Wreccics  
 Morrow County Planning Department  
 Coordinate System: NAD83 Oregon GIC Lambert ft  
 Datum: North American 1983  
 Projection: Lambert Conformal Conic  
Maple 5 Planning/Land Use/Information/Map/Map 4N 25E 15 TL1000



**PRELIMINARY FINDINGS OF FACT  
HARDSHIP VARIANCE REQUEST  
HV-N-044-22**

**REQUEST:** Approve a recreational vehicle as a special use temporary hardship dwelling to provide close-at-hand care to the property owner.

**APPLICANT:** Glenda Bahler  
78651 Olson Road  
Boardman, OR 97818

**OWNER:** Gary Klinger  
78651 Olson Road  
Boardman, OR 97818

**PROPERTY DESCRIPTION:** Tax lot 1001 of Assessor's Map 4N 25E 15

**PROPERTY LOCATION:** Southeast of Boardman on Olson Road, near Kunze Lane.

**I BACKGROUND INFORMATION:**

The request is to approve one (1) temporary hardship dwelling to allow close-at-hand care for Gary Klinger. The subject property is located in the Farm Residential (FR2) zone outside of the Boardman Urban Growth Boundary.

**II APPROVAL CRITERIA:**

The criteria for the Variance Request is found in the Morrow County Zoning Ordinance Article 7 Variances Section 7.300. Criteria are listed below in bold type, followed by a response in standard type.

**SECTION 7.300 Special Uses**

**Morrow County may allow the following land uses that may or may not be specifically listed as an allowed use in a designated zone. When considering a request for a Special Use the Planning Commission will use the specific criteria to balance whether the detriment to the local community caused by granting a Special Use is outweighed by the benefit to the property owner and/or the larger community. Any change in use, relocation or expansion would require a new or amended use authorization.**

**MEDICAL HARDSHIP. A medical hardship is a Special Use of a manufactured home, recreational vehicle or an existing building necessary for a relative or other designated caregiver to care for or provide custody for an elderly, mentally handicapped, or infirm person whom a medical professional certifies needs this kind of care or custody. This certification will be on the medical professional's stationery or stamped by the medical professional's office, and will indicate that the patient is not physically or mentally capable of maintaining himself/herself in a residence on a separate property and is dependent on someone being close by for assistance. As an alternative, the medical professional can stamp and sign the application form available through the Planning Department for a medical hardship. Financial hardship conditions, child care, and other convenience**

HV-N-044-22

Findings of Fact

arrangements not relating to physical and/or mental impairment are not considered an infirm condition.

The provisions of this section are to apply when the proposed use does not qualify as a continuation of a nonconforming use, not permitted by right, nor permitted through the operations of other more pertinent procedures and provisions of this zoning ordinance. Medical hardship Special Use permits for dwellings are not to be construed, permitted nor utilized as a means to abrogate the intent, purpose or procedures of the County's Comprehensive Plan or Zoning Ordinance regulations.

No medical hardship Special Use permit shall be granted that would have the effect of creating a permanent zone change or result in a hardship when the use is not permitted to continue at the expiration of the permit periods. Further, no medical hardship Special Use permit will be granted which has the effect of conferring a special privilege for which other property within the same zone would not be equally eligible.

The applicant has been provided with the procedures and standards and does comply with the intent of this Section. The use does not qualify as a continuation of a nonconforming use as it is a permitted variance for hardship reasons as allowed by Morrow County Code. This temporary hardship permit will not have the effect of creating permanent zoning as when the hardship variance is no longer needed the dwelling is required to be removed. This permit does not confer a special privilege for which other properties within the same zone are equally eligible when they meet the applicable criteria for approval.

- A. As a medical hardship Special Use in any zone that allows dwellings, the Commission may allow as a Special Use one manufactured home, recreational vehicle, or temporary use of an existing building complying with the standards of Section 4.110, as applicable, and providing that no additions, except approaches or handicapped ramps, to the temporary residence shall be permitted in conjunction with a primary dwelling with the following findings:**
- 1. That an accessory dwelling is necessary to care for or provide custody of an elderly, mentally handicapped, or infirm person who a medical professional certifies needs this kind of care or custody as required in A. above.**  
A signed doctor's statement was received with the application. The medical professional states that Mr. Klinger requires family assistance do to him suffering from numerous medical conditions.
  - 2. Electric, water and sewer utility connections shall be made to the temporary residence. If the medical hardship dwelling will not use a public sanitary sewer system, the dwelling shall use the same subsurface sewage disposal system used by the existing dwelling if that disposal system is adequate to accommodate the additional dwelling or as otherwise allowed and conditioned by the Planning Commission.**  
If the applicant desires to connect the RV to water, domestic water would need to be obtained through a shared or new well. This is listed as a condition of approval.

**HV-N-044-22**

**Findings of Fact**



Prior to the connection of the RV to the existing onsite septic system it is listed as a Condition of Approval that the applicant obtain authorization from the Umatilla County Public Health.

3. **Within 90 days of the end of the medical hardship, the manufactured dwelling or recreational vehicle shall be removed or, in the case of an existing building, the building shall be removed, demolished, or returned to an allowed non-residential use.**

Planning staff would recommend and list as a Condition of Approval that the applicant notify the Planning Department when Mr. Klinger is no longer in need of the hardship as the hardship dwelling must be removed within 90 days of the hardship ending.

- B. As a medical hardship Special Use in a resource zone, the following are also applicable:**

1. **That the medical hardship dwelling use will not force a significant change in accepted farm or forest practices on surrounding lands devoted to farm or forest use;**
2. **The medical hardship dwelling use will not significantly increase the cost of accepted farm or forest practices on lands devoted to farm or forest use.**
3. **The landowner for the hardship dwelling shall sign and record in the deed records for the County a Right-to-Farm or a Right-to-Forest Statement binding the landowner and the landowner's successors in interest, prohibiting them from pursuing a claim for relief or cause of action alleging injury from customary farm or forest practices.**

These requirements are not applicable as the subject property is zoned Farm Residential.

- C. A medical hardship Special Use permit granted under this section is void when the elderly, mentally handicapped, or infirm existing resident or other person who is the subject of the permit no longer needs care, moves to another residence, is absent from the residence for more than 120 days or leaves the residence with no likelihood of returning for continued residency of at least 30 days. Exception to the 120-day limit can be provided for in the case of extraordinary circumstances such as extended hospitalization. These extensions can be approved by the Planning Director for up to an additional 60 days without Planning Commission approval. Additional extensions will require Planning Commission review and approval.**

Planning staff would recommend and list as a Condition of Approval that the applicant notify the Planning Department if Mr. Klinger is not in need of the hardship approved under this request for more than 120 days to determine next steps under this approval.

- D. The County Planning Director or designee may review permits issued under this section at any time and may revoke permits when they are found to be out of compliance. After the initial approval by the Planning Commission any required renewal shall be applied for as a medical hardship extension. The decision to approve a medical hardship extension shall be an administrative decision of the Planning Director.**

Should the Planning Commission approve the request and the applicants seek renewal before the initial permit expires, the decision will be administrative and not have to be heard in front of the Planning Commission. Any Code

**HV-N-044-22**

**Findings of Fact**

Enforcement action or failure to comply with the conditions of this permit would put this permit at risk of being revoked.

- E. Any dwelling authorized by a medical hardship Special Use permit must be located as close as possible to the primary dwelling. Unless there are physical limitations of the land this should be within 100 feet of the primary dwelling.**

The Recreational Vehicle is proposed to be within 100 feet of the primary dwelling. This criterion is met.

- F. County Zoning and Building Permits will be required. A Rural Address will also be required to facilitate emergency response.**

Planning staff would recommend and list as a Condition of Approval that the applicant obtain appropriate Zoning and Building permits. Also listed and recommended as a Condition of Approval is for the applicant to apply for a Rural Address for the hardship dwelling in conjunction with the Zoning Permit.

- G. A medical hardship Special Use permit is valid for up to 2 years from the date of initial issuance, i.e., permits issued in an odd-numbered year will expire in the next odd-numbered year. All permits will have an expiration date of January 31. The County will process all medical hardship Special Use permit renewal requests once per year in January. The County will give permittees not less than 30 calendar days written notice of the pending expiration of their Special Use permits, advising that a renewal will be required. Failure to receive notification of pending expiration does not constitute an extension of time for the permit. The Planning Director shall not renew the medical hardship Special Use permit until the permittee has shown compliance with the conditions for issuance specified in this Section at the time of renewal and the County has received evidence of the continued validity of the medical hardship.**

If approved, this permit is valid through January 31<sup>st</sup>, 2024. Notice of renewal opportunity will be sent to owner at least 30 days prior to expiration date.

**III PUBLIC NOTICE PUBLISHED:** March 8, 2022  
East-Oregonian

March 9, 2022  
Heppner Gazette-Times

**IV AGENCIES NOTIFIED:** Bev Bridgewater, West Extension Irrigation District; Mike Hughes, Boardman Fire District; Mike Gorman, County Assessor; Eric Imes, Morrow County Public Works; Glenn McIntire, Building Official; Brandilyn Bridges, Umatilla County Public Health

**V PROPERTY OWNERS NOTIFIED:** March 8, 2022

**VI HEARING DATE:** March 29, 2022  
North Morrow Government Center  
Irrigon, Oregon

**VII DECISION OF PLANNING COMMISSION:** Recommendation of staff is to approve the application subject to the following conditions.

1. Domestic water be obtained either through a shared well or a new well.

**HV-N-044-22**  
**Findings of Fact**

2. Obtain approval from the Umatilla County Public Health for the hardship dwelling to use the existing subsurface sewage disposal system if that disposal system is adequate to accommodate the dwelling before Zoning permits are issued.
3. Notify the Planning Department when Mr. Klinger is no longer in need of the hardship variance permit as the hardship dwelling must be removed within 90 days of the hardship ending.
4. Notify the Planning Department if Mr. Klinger is not in need of the hardship variance permit approved under this request for more than 120 days to determine next steps under this approval.
5. Obtain appropriate Zoning and Building permits.
6. Apply for a Rural Address for the hardship dwelling in conjunction with the Zoning Permit.
7. This hardship variance permit is valid until January 31, 2024.

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Jeff Wenholz, Chair

Date

ATTACHMENT:  
Vicinity Map

# Vicinity Map



**HV-N-044-22**  
 Glenda Bahler  
 4N 25E 15 TL1001

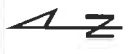
**Legend**  
 Tax Lot

- Urban Growth Boundary
- Subject Property



Date Saved: 3/4/2022 9:48 AM

Cartography By: Stephen Wreccics  
 Morrow County Planning Department  
 Coordinate System: NAD83 Oregon GIC Lambert ft  
 Datum: North American 1983  
 Projection: Lambert Conformal Conic  
Header is Missing: Morrow County Project 22-00000001





## **MEMORANDUM**

To: Morrow County Board of Commissioners  
 From: Tamra Mabbott, Planning Director  
 CC: Planning Commission  
 BOC Date: February 9, 2022  
 RE: Monthly Planning Update

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### **Planning Commission Update**

On January 25, 2022 Planning Commission held their first meeting in the new County North Morrow Annex Building in Irrigon. Staff hosted a light meal and a tour of the new building. Four applications were approved; one application was continued to February. Although permanent media equipment is on back order and not yet installed, staff were able to utilize older equipment so that the meeting was available for participation in person and on zoom.

Everyone is enjoying the beautiful new building. We still have a few boxes to unpack and furnishings to order but the new work space is appreciated by staff and customers.

### **Current Planning Activity January 2022:**

- Zoning Permits – 8
- Land Use Compatibility Reviews – 7
- Rural Addresses – 2
- Agriculture Building Permit Exemption – 3
- Land Partitions – 3
- Replats - 2
- Variance – 1
- Conditional Use Permit - 1

### **Columbia River Heritage Trail**

The Eastern Oregon Visitors Association (EOVA) trail coordinator is coordinating funding assistance for trail projects in the EOVA region which includes most of eastern Oregon. The trail marker and signage inventory report will be ready to share with Planning Commission at their February 22<sup>nd</sup> meeting. Folks interested are encouraged to contact the Planning Department. The final version will also be posted on the website.

### **Natural Hazard Mitigation Plan (NHMP)**

County will soon begin working on the 2022 update of the NHMP. Updates are required every 5 years. Paul Gray, Emergency Manager, will be the lead. Pam Reber, Natural Hazards Planner with the Oregon Department of Land Conservation & Development (DLCD) will be the state agency lead. Federal Emergency Management Agency (FEMA) awarded funds for the NHMP Update.

**Tourism**

After responding to Request for Proposals to provide support and advocacy for tourism promotion in Morrow County, the Board of Commissioners entered a contract with Karie Walchli who has provided similar services in Umatilla County for 12 years. Planning Director and Board Chair Doherty met with Ms. Walchli to kick off her work in Morrow County. She will be working with chambers of commerce and any group interested in tourism development in Morrow County. Anyone interested may contact Karie at: [karie.walchli@gmail.com](mailto:karie.walchli@gmail.com)

Speaking of tourism, Planning staff developed an informational brochure targeted at landowners and enterprising individuals interested in starting an agri-tourism business. The brochure is attached and will be on the department website soon.

**Energy Projects**

Staff hosted three pre-application meetings for new solar projects. Staff helped coordinate an informational session with Oregon Department of Energy and Idaho Power Company. Status of renewable energy projects in Morrow County can be found here: <https://www.co.morrow.or.us/planning/page/renewable-energy-1>

**Grants Round Up:**

1. The first of two planning grant projects for the Willow Creek Valley will begin in mid-March. Staff and consultant ECONw, an economics, finance and planning firm are coordinating dates to begin the work. Residents of Ione, Lexington and Heppner interested in either project please contact your city hall or County Planning Department.
2. Staff continues to work on a planning grant application with the US Navy to codify and implement recommendations of the Military Economic Advisory Committee (MEAC). The scope of the project has been expanded to address Statewide Planning Goal 13 Energy Conservation

**Water and Land Use**

Planning Director, serving as the land use planner representative on the Division 10 Rule Advisory Committee (RAC) with Oregon Water Resources Department has continued review. The purpose of the RAC is to review existing Administrative Rules for designating new Critical Groundwater Areas (CGWA). New rules would then be forwarded to the Oregon Water Resources Commission (OWRC). Morrow County has three designated CGWA areas and one classified area. The CGWA designation imposes significant restrictions on water use and thus impacts land use. Several other areas in the state have requested a CGWA designation, including Harney County and Klamath County.

**Code Enforcement**

New compliance Planner, Katie Keely, completed renewals of hardship permits and has begun the work to review Home Occupation permits. Ms. Keely has been busy in the field and in the office.

**Other**

Two staff members are participating in a 12-week leadership training by the National Association of Counties (NACO). Tamra Mabbott, Planning Director, and Stephanie Case, Planner II recently completed part one of a four-part course along with nine others from Morrow County.

Planning Staff have been working a comprehensive inventory of all Ordinances approved by county. Look for that on the website soon.

**Planning Trivia Question of the Month:** Who owns land adjacent to the Columbia River in Morrow County?

**Answers to last month's trivia question:** How many square feet in an acre? How many acres in a square mile? There are 43,560 square feet in an acre of land and 640 acres in a square mile.



## **MEMORANDUM**

To: Morrow County Board of Commissioners  
 From: Tamra Mabbott, Planning Director  
 CC: Planning Commission  
 BOC Date: March 9, 2022  
 RE: Monthly Planning Update

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### **Planning Commission Update**

Planning Commission held their February 22<sup>nd</sup> meeting in Boardman at the Bartholomew Building. Off-site participation was also available via Zoom. Five applications were approved; one application was continued to March.

### **Current Planning Activity February 2022:**

- Zoning Permits – 6
- Land Use Compatibility Reviews – 15
- Rural Addresses – 2
- Agriculture Building Permit Exemption – 3
- Land Partitions – 1
- Hardship Variance – 1
- Appeal – 1
- Code Enforcement Hearing - 1

### **Columbia River Heritage Trail**

During the February Planning Commission meeting staff presented a draft sign plan for the Heritage Trail. The plan is a work in progress and is intended to include a description of all existing trail markers and interpretive panels. Once complete, department staff will order replacement signs and placards and will coordinate with County Public Works and other agencies on installation. Where posts have been tipped over or removed staff will work with a consultant to prepare environmental review required by Army Corps of Engineers (ACOE), the underlying landowner for most of the trail. Staff will then begin conversations with public agencies and private interested parties about content material for the interpretive panels, many of which have been damaged by weather and vandalism. Public involvement is highly encouraged, for both the sign project and the interpretive panels are encouraged to contact the Planning Department. Final version of the sign plan will be shared with Planning Commission and Board of Commissioners at a later date.

### **Natural Hazard Mitigation Plan (NHMP)**

Paul Gray, Emergency Manager, is leading the NHMP update. Pam Reber, Natural Hazards Planner with the Oregon Department of Land Conservation & Development (DLCD) is the state agency lead. Stephen Wrecsics, GIS Planning Tech is providing mapping and other technical assistance.

### **Tourism**

Karie Walchli began in earnest her work as Tourism Coordinator for Morrow County. Her contract is an exciting new initiative of the Board of Commissioners to enhance and coordinate tourism opportunities in Morrow County. Ms. Walchli was the guest speaker at Heppner Chamber on March 3<sup>rd</sup>.

Ms. Walchli is also hosting two sets of meetings. Tuesday, March 8 at Sage Center, Boardman, 4:00 pm, is a meeting about the regional farm trails, River to Hills Farm Trail and the Whisky & Rocks Farm Loop. Anyone interested is encouraged to attend. See attached flyer.

As part of Ms. Walchli's work on a countywide tourism strategy she will be hosting two meetings. The first meeting is March 31<sup>st</sup> at 7:00 in the Gilliam Bisbee building in Heppner. Dessert will be provided by Heppner Chamber. Logistics are pending for a second tourism strategic planning meeting to be held in Boardman. Anyone interested may contact Karie at: [karie.walchli@gmail.com](mailto:karie.walchli@gmail.com)

### **Energy Projects**

Four new solar projects in various pre-application phases are on slate for Morrow County Planning Commission. The first complete application may be heard as early as May 2022. List of pending and approved projects can be found here: <https://www.co.morrow.or.us/planning/page/renewable-energy-1>

### **Data Centers in Morrow County**

Morrow County currently has six data center campuses, including one inside the Boardman City Limits. A 7<sup>th</sup> data center campus is under permit review by planning staff. Stephen Wrecsics, GIS Tech is working on a map of the data center campuses in Morrow County.

### **Grants Round Up:**

1. The Housing Implementation Plan project for Willow Creek Valley will have a kick off meeting March 15<sup>th</sup> with city and county staff and consultant ECONw, an economics, finance and planning firm. A schedule of public meeting dates will be released later. Residents of Ione, Lexington and Heppner communities interested in the project please contact City Hall or County Planning Department.
2. A second grant project, the Goal 9 Economic Development plan for Ione, Lexington and Heppner will begin late summer. County and city staff will start the consultant selection process later this Spring.

### **Water and Land Use**

Planning Director is serving as the land use planner representative on the Division 10 Rule Advisory Committee (RAC) of the Oregon Water Resources Department. Based on public comments and concerns about the scope and intent of the draft rules, work has been paused. A new meeting will be scheduled. has continued review. The purpose of the RAC is to review existing Administrative Rules for designating new Critical Groundwater Areas (CGWA).

In terms of water quality, planning staff are supporting a new OSU research project for a new sub-committee co-chaired by Morrow County Commissioner Lindsay and Commissioner Dorrان of Umatilla County. The research is aimed at better understanding the sources of nitrates in the Lower Umatilla Basin Groundwater Management Area (LUBGWMA). Stephanie Case, Planner II, serves on the primary LUBGWMA committee.

### **Other**

Tamra Mabbott, Planning Director, and Stephanie Case, Planner II continue with the 12-week leadership training by the National Association of Counties (NACO) along with nine others from Morrow County.

**Planning Trivia Question of the Month:** How many people serve on the Morrow County Planning Commission?

**Answers to last month's trivia question:** The Army Corps of Engineers (ACOE) owns most of the land adjacent to the Columbia River in Morrow County. Port of Morrow is also a major landowner.





# Morrow County

Exclusive Farm Use Zone

Agri-Tourism Opportunities

Morrow County Planning  
Department

29.5 NE Main Ave.

Irrigon, Or 97844

Phone: 541-922-4624

[www.co.morrow.or.us/planning](http://www.co.morrow.or.us/planning)

## What is Agri-tourism?

In 2011 the Oregon Legislature passed Senate Bill 960 (SB960) allowing counties to permit agri-tourism and other commercial events or activities.

Morrow County has adopted regulations to allow Exclusive Farm Use property owners to hold a permit for a certain number of events for profit each year on their property.

## What kind of permit is required for Agri-tourism events?

Morrow County Planning makes land use decisions for Agri-tourism events. There are three types of Agri-tourism permits as summarized here:

“Single Event” licenses for one event each calendar year for a maximum of 72 consecutive hours, up to 500 attendees, and onsite parking for 250 vehicles.

“Six Events” Limited Use Permits for up to six events in a calendar year for a maximum of 72 consecutive hours per event and may be valid for two calendar years.

“18 Events” Limited Use Permits for up to 18 events each year on a parcel of at least 160 acres in size. Events are incidental and subordinate to the existing commercial farm use of the parcel and necessary to support commercial farm uses or agricultural enterprises in the area.

## How to obtain an Agri-tourism permit?

Morrow County Planning issues Land Use Permits for Agri-tourism events. You can download a Land Use Request Application from planning’s web page at [www.co.morrow.or.us/planning](http://www.co.morrow.or.us/planning) or request the application from the planning office. You will also need to complete a Supplemental Application addressing information specific to the proposed event. Planning staff are available each day from 9am-5pm and are happy to help.

## What happens after I submit the application?

The Planning Director shall review the application and all information submitted with it and evaluate whether the application is complete enough to process. Within 30 days of receipt of the application, the Planning Director shall complete this initial review and issue to the applicant a written statement indicating whether the application is complete enough to process, and, if not, what information must be submitted to make the application complete.

## What type of conditions will be placed on Agri-tourism events?

Specific Conditional Use Permit Standards for Agri-tourism events are listed in the Morrow County Zoning Ordinance (MCZO) section 3.010 (22)(J). Along with limitations on the number of events, event hours, vehicles and attendees, parking, additional standards that may apply include:

- Events must be incidental and subordinate to the existing farm use on the tract of land.
- Events must occur outdoors, in temporary structures, or existing permitted structures, subject to fire and life safety requirements.
- Events must not cause the land to be altered, such as grading, filling or paving, etc.
- Events in combination with other Agri-tourism events, commercial events or activities authorized in the area shall not alter the stability of the area land use pattern.

## What can I do without an Agri-tourism permit?

If you are operating a farm in an EFU zone you may have u-pick, u-cut, hay rides and conduct activities for children such as picking pumpkins from a pumpkin patch. These activities generally are part of the commercial farming operation and do not require a permit.



## What about farm stands?

The main purpose of a farm stand is the sale of farm crops and livestock grown on the farm, or grown on other farm operations in the local agricultural area. Farm products may include both fresh and processed items such as jams, syrups, apple cider, cheese, cured meats and similar farm crop and livestock products, but not prepared foods.

Incidental items and fees from promotional activities are allowed and may not make up more than 25 percent of the total annual sales from the farm stand. Incidental items could include food utensils, hats, sunscreen, etc.

The farm stand structure must be designed and used for the sale of farm products and may not include a structure for occupancy as a residence or for other activities for banquets, public gatherings or public entertainment.

## What kind of permit is required for a farm stand?

Morrow County Planning issues a Zoning Permit for constructing and operation of a farm stand. The Zoning Permit requires a site plan review and may be issued over the counter while the applicant waits. You can download a Zoning Permit from the Planning Department web page at <https://www.co.morrow.or.us/planning> or request the application from the planning office.

Join us for a

# Farm Trail Focus

## Kicking off the 2022 Season

★★★★ The ★ ★ ★

**RIVER TO HILLS**

**FARM TRAIL**

★★★★ ★ ★ ★ ★

*Eastern Oregon*

Tuesday March 8th, 2022

4:00 pm

Sage Center in Boardman, OR

Incase you are unable to  
make it in person,  
see email information  
to join us by zoom.

★★★★ The ★ ★ ★

**WHISKY & ROCKS**

**FARM LOOP**

★★★★ ★ ★ ★ ★

*Eastern Oregon*

Wednesday March 9th, 2022

10:30 am

Public Library in Milton-Freewater, OR